

City of East Bethel
Planning Commission Agenda
7:00 PM
Tuesday, April 26, 2016



Agenda

		<u>Item</u>
7:00 PM		1.0 Call to Order
7:02 PM	pg. 1	2.0 Adopt Agenda
7:03 PM	pg. 2-15 pg. 16-25	3.0 Approval of Meeting Minutes February 23 rd , 2016 (partial) – Regular Meeting March 22, 2016 – Regular meeting
New Business		
7:05 PM	pg. 26-27	4.0 Concept Plat Minor Subdivision – Request to consider a concept plat for a minor two lot subdivision for Rimma Medelberg at 20381 East Bethel Blvd., PID # 223323110006; Zoning RR, Rural Residential
7:15 PM	pg. 28-102	5.0 CST Site Plan review Request by Chad and Megan Toft, CST Companies, LLC to build and locate a warehouse, trucking, and packaging facility at 23805 Highway 65 NE, PID# 293423430012, Zoning I1, Light Industrial
Reports		
8:45 PM		6.0 City Council
8:55 PM	pg. 103-110	7.0 Other Business
9:00 PM		8.0 Adjournment

EAST BETHEL PLANNING COMMISSION MEETING

FEBRUARY 23, 2016

6.0 CST Distribution Concept Plan

Ms. Winter stated the next item before you is a proposed business relocation to 237th Avenue and Highway 65 for a company, CST Distribution, LLC. The applicable Code section is Appendix A, Zoning, Light Industrial and Section 4, Article 12, as well as several other sections within our Code. This evening is really a review and comment on the proposed relocation of CST to East Bethel.

Ms. Winter stated CST Distribution, LLC and CST Transportation, Inc., are owned by Chad & Megan Toft. CST Distribution, LLC is a wholesale distributor of softener salt, mulch, ice melt, firewood, washer fluid, and bottled water and also a contract packager of primarily mulch and soils. CST Transportation, Inc. is a local/regional transport trucking company, specializing in forklift mounted flatbed trucks, with occasional over-the-road capabilities. Customers include Menards, Home Depot, Cub Foods, and SuperAmerica stores among others.

Ms. Winter stated CST is proposing to construct up to a 32,000 square foot warehouse/office facility and a 10,000 square foot bagging plant. The property is the Mike Wyatt property at 237th and Highway 65, which is a 40-acre parcel. The mulch will be stored and dyed outside during the winter months. By June, the majority of the mulch piles and pallets are gone. They do not process trees into mulch but the material is shipped in, dyed and bagged on site.

Ms. Winter stated CST's proposed business use as a production, distribution, and warehouse facility is consistent with the zoning for the site at 237th and Highway 65. Article 12 in our Code requires a Site Plan Review prior to the issuance of any building permits to ensure safe, functional and attractive development. This Plan will be submitted to the Planning Commission and the City Council for approval. Tonight's discussion is a preliminary discussion and the formal Site Plan will be submitted to the Planning Commission at the regular meeting in March for approval.

Staff has met with the owners of CST Distribution and discussed with them the requirements of a formal Site Plan Review. City staff has also toured their facilities in Rogers and Elk River. City staff has made them aware of the following:

- Visual Impact upon the immediate neighborhood and the need to provide adequate screening.
- Environmental issues including, but not limited to, groundwater drawdown, treatment of dyeing effluent, stormwater runoff, noise, odors, control of site debris.
- Traffic issues relating to truck impact on 237th Avenue, entrance locations and potential stacking issues, peak traffic concerns, as well as need for by-pass lanes or need for right-in right-out only.

Ms. Winter stated I should comment, the Site Plan itself will be reviewed by Anoka County Highway Department because this is on a County road. Therefore, the Highway Department will ultimately have the say as far as what they are going to be required to provide for access to this location.

Ms. Winter stated the Site Plan process does cover the issues as stated along with signage, lighting, and landscaping. A formal Site Plan Review does not require a public hearing; however, the Roads Commission and EDA have also reviewed this project and their recommendations will be reflected in the final submission to City Council .

Ms. Winter stated included in your packet is a fact sheet about CST; a Site Plan, which I have up on the board and I can explain a little bit more about that; a location map; as well as some photos of their current operation and where it's located, which I can also provide to you as we get through this process. So that's what I have so far Mr. Chair.

Chair Plaisance stated very good. At this time, we will have comments from the Commission. Discussion?

Mr. Holmes asked are we going to eventually have a service road alongside 65? Ms. Winter replied the Roads Commission, at their last regular meeting, had talked about the need for an additional service road in this area. That service road that would be proposed, in all likelihood, would be a continuation of Davenport, which is right here. *(Ms. Winter pointed to the location on an overhead slide.)* So the service road, it's a little hard to see and I apologize for that, but it's right in this location. It would continue up and eventually connect with Cemstone.

Mr. Holmes asked so it goes straight north instead of turning like... Ms. Winter stated right, there is an existing driveway that exists on this property. That's located right here.

Ms. Bonin asked how far from Cemstone is that? Ms. Winter asked this? Ms. Bonin answered yeah. Chair Plaisance stated it is quite a ways. Ms. Allenspach stated yeah, I was going to say one mile.

Ms. Winter stated in a very preliminary discussion with Anoka County there existing entrance simply wasn't going to work not only for their truck traffic but just simply it wasn't going to work for the site. So we know that there's going to be a requirement to have a second access. Again, the formal Site Plan has not been submitted to the County but in very preliminary conversations we've had with the County, they've indicated there's got to be two different access points for this property.

Mr. Terry stated semis, two semis, would stack that back into the intersection. Ms. Winter stated correct. Ms. Allenspach stated one might.

Chair Plaisance stated access, Colleen that you have going through the middle of the property. *(Ms. Winter referenced an overhead slide.)* Ms. Winter asked right here? Chair Plaisance stated yes. Is that going to extend to other properties? Or, is this just for their property? Ms. Winter stated it's for their property for the time being. It's in our Comp Plan to continue north as those properties develop. So if the properties to the north of here someday develop, then that road would continue as part of that service road.

Chair Plaisance stated so I'm seeing, according to this plan, that you have pallet storage on the opposite side from where the main business is being located. Ms. Winter stated correct. Chair Plaisance asked is that going to be an issue? Moving things from one side of the road to the other if that does continue? Ms. Winter stated I guess that's part of the conversation, that as a Planning Commission, you're going to want certainly to talk about.

Ms. Winter stated maybe let me go through this real briefly if I could please. So we talked about the service road or the potential road here. There's another access, and again, this is all very preliminary, that potentially is here as well. Those, as we talked about, the access points, will be determined by the County. So, that's just, we know there has to be two of them.

(Ms. Winter referenced overhead slides.) Ms. Winter stated the proposed building, one of the buildings, the production and warehouse facility, would be located here and this would include the office as well. Then located right here would be their bagging operation, which is the 10,000 square foot building. The pallet storage would take place on this portion of the lot, back here. Their mulch dyeing operations would be right in this area. Along the front of the property, on the south side, there would be a berm from this point all along this side, all the way up to Davenport and on the other side of Davenport. Again, it's difficult to see. If anybody wants, there are maps up here as well, at the podium, that outline the Site Plan. Then the berm itself, you can kind of see, it's really hard to see in this drawing, but it would be landscaped and at an elevation where it would adequately screen so that there wouldn't be a sight line from this side of the road.

Ms. Winter stated the other thing that is proposed at this point, is these are two different holding ponds that are being proposed. One being an infiltration type of pond. And then they are going to have to go through the joint application because there is a wetland here as well as a wetland up here. So there are two known wetlands on the property. So, they will have to go through the joint application affecting waterways as well for further delineation of where those wetlands are located and whether or not they'll have any impact on the wetlands. In addition, as we talked before, they do use between 30,000 and 40,000 gallons of water a day when they are dyeing the mulch. So, they are going to have to go through the process of dealing with what is required for permitting for a large water user per MNDNR rules.

Mr. Holmes stated Colleen, before you do that, that front area to the west that's going to be display area, is that correct? The farthest one to the west? Chair Plaisance asked where it sticks out towards the road? Mr. Holmes stated yeah, where it sticks out. Ms. Winter stated their future plan would be to have a retail display area so they would have some retail at that location as well. Mr. Holmes stated okay.

Ms. Winter stated there is a house on this property and the proposal would be to tear down that existing house. Mr. Cornicelli asked so that parcel includes that old house that's in that, north of Coopers, north of the Liquor store? Ms. Winter answered the house north of the one by the liquor store. It's the white one. Mr. Cornicelli stated right.

Chair Plaisance asked so they are going to be bringing in the mulch from another location? Ms. Winter answered yes. Chair Plaisance asked they are not mulching on the property, correct? Ms. Winter answered no, it's just bulk, coming in bulk and they would be dyeing it and bagging it.

Ms. Winter stated this picture that I'm showing you right here is their existing operations. It's just a little bit of an overview. Their current location, they're currently in two different locations right now in Elk River and Rogers. The idea is they want to be able to combine. They've run out of space at both of those locations. They're very interested in bringing all their facilities into one location. Included in your packet was the fact sheet that we had talked about. How many employees they were going to have, that type of thing.

(Ms. Winter referenced an overhead slide.) Ms. Winter stated so the mulch piles, you can see what they look like here. And then this is their actual machine that they use to dye the mulch. That's all sitting on pavement. Then this is just another picture looking at their yard as far as what it would look like.

Mr. Terry asked how is the runoff from that dyeing process treated? Ms. Winter stated we don't know at this point. Obviously, it has to be treated and it has to be done in such a way that it's environmentally safe.

Ms. Bonin asked can it be reused? Ms. Winter stated I don't know. Ms. Bonin stated I would think they could reuse some of that water rather than getting rid of all of it.

Mr. Terry asked do we know what their daily truck traffic count would be? Ms. Winter answered anywhere from 20 to 40 trucks. Mr. Terry asked that's in and out? Or, out one way and then again in at the end of the day? Ms. Winter stated that's a good question. They currently have 14 trucks that are parked at their facility. They operate 27 trucks. So, it would appear that the trucks would be there loading, unloading, and going back out. The actual number of trucks that are parked there would be 14. Then the other trucks are disbursed throughout the cities.

Ms. Allenspach stated and Colleen, if you wanted to put that first concept plan back up, it shows where they park their trucks along the north edge. It's got a row. Chair Plaisance stated yes, the northwest side. Ms. Allenspach stated where they're all parked along. They're not just, like, all over the property. Ms. Winter stated yes. Here's the location for where they're proposing to park their trucks. It's in this section right here. Ms. Allenspach stated thank you.

Ms. Allenspach asked around the pallet storage areas, are they planning any kind of fencing or buffer? Or, is it not necessary? Ms. Winter stated according to what our requirements are, they are required any time you are adjacent to residential areas you are required to provide screening. In addition, they have talked about from a security standpoint fencing in some of these areas as well for security purposes. Ms. Allenspach stated that's what I wondered, Thanks.

Mr. Terry stated the 30,000 to 40,000 gallons per day, is that only during the dyeing

process? Ms. Winter answered correct. Mr. Terry asked and do you know what, how long in the year that occurs? Ms. Winter answered I believe it's right around six months. Mr. Terry stated six months. It's hard for me to quite grasp the scale of that. But, is that a potential risk to the ground water supply? Ms. Winter stated I think that's one of the questions that needs to be asked. I don't know that we know that answer at this point. I can tell you that there are some other large users in that same area. You have the Wyatt property that's just to the north of there that's a large user of water as well. Obviously, the residences on the south side aren't maybe large users but combined, utilize a significant amount of water. Mr. Cornicelli stated that's 5.5 million gallons, rough math.

Mr. Terry asked is there any evidence of any problems with the water table in the locations where they're at? Ms. Winter stated I guess I don't know the answer to that.

Mr. Holmes stated the question I have, we already have a company in East Bethel that does the dyeing and stuff of the wood. I don't know how many times I've heard about the smell and stuff from that. Is that going to be the same problem here? Because I know a lot of residents sure don't like the smell from that. Mr. Terry asked is it from the dyeing? Because they do the mulching there. Mr. Holmes stated yeah, they do the mulching and I don't know if the smell is from the mulching or from the dyeing, or whatever. Ms. Winter stated in that case, I would imagine it's more from the mulching side of it. Mr. Holmes stated I don't know but there's a lot of people that sure don't like the smell and I can understand that.

Ms. Bonin stated the real question is about where the traffic is going to be. Is it going to be on 237th? All those trucks? Mr. Holmes stated sure. Ms. Winter answered yes. Chair Plaisance stated out to 65. Ms. Bonin asked will anybody want to use that convenience store area with all that traffic?

Chair Plaisance stated if you want to make a comment, you will have to come forward and speak your name and your address. Mr. Cornicelli asked have we done public comment?

Troy Strecker, 23673 Baltimore Street, stated right across the street from Coopers, behind the church there. And, you're right. With all the semis, it's already a dangerous intersection. I leave there every morning. I come in at 5 o'clock in the evening on the way back. And, I don't know how many times, going east or west, I've almost gotten rear ended either coming in or out of that gas station and the liquor store itself. If those semis are coming out of there and they're parked there, it's only going to add to the problem of them, with people waiting to turn. And, with the sunlight, it's already a bad area.

Mr. Strecker stated I do agree with you. I don't necessarily believe that the smell is only from the mulching process. If you stack 500,000 pounds of wet wood, dirt, all that kind of stuff, that creates a smell. And, as anybody lives in that area knows, there's a wind tunnel that comes right across Highway 65 and all the way through there. In my own personal opinion, living there for 15 years now, that wind is going to come right across there and it's going to blow all that smell into all of our residential areas and our neighborhoods.

Mr. Strecker stated my wife does daycare right across the street from there and I am a little concerned with the fact that she's with the kids outside most of the days during the summer.

Chair Plaisance stated I hate to interrupt you at this point, but the point is that this is really not a public hearing to hear everyone's statement. You made a comment, which is why I kind of wanted you to come up and state your name. There is going to be a meeting, when is that, tomorrow Colleen? Ms. Winter stated tomorrow night, correct. Chair Plaisance stated with City Council, has a Special Meeting So, if you have some concerns, I would recommend coming to that meeting instead. This meeting is basically to have us discuss the difficulties going on here.

Mr. Strecker stated well, that's kind of part of the problem. I know most of the people here with me didn't know anything about this meeting and didn't know what this was about. So, that's why we came tonight, thinking this was that forum. So, I apologize. Chair Plaisance stated no, that's okay. I just wanted to clarify what this meeting is about and if you all came here tonight expecting to speak on this, I do apologize. But, this is not the open forum public meeting.

Ms. Winter stated the meeting is at 6 o'clock tomorrow, just to clarify that. Again, what we stated is this is a permitted use in that district. So when you have a permitted use, something that's a permitted use in a Zoning District. You're not required to go through a public hearing if they are not going to be further subdividing the property, asking for a Conditional Use Permit, or asking for a Zoning Amendment. But they do have to go through what's called a Comprehensive Site Plan Process. That's what I had outlined before. Chair Plaisance stated thank you.

Ms. Bonin asked what time is the meeting tomorrow night? Ms. Winter answered 6 p.m. Ms. Bonin stated 6 o'clock. I guess even though that's what the meeting is for, my personal opinion is with this many people here, I would like to hear some of their comments. Ms. Winter stated it's up to you Mr. Chair.

Chair Plaisance stated at this point, Colleen, since this is not a public forum, I'm going to put it to a vote to the Members to ask if they are willing to hear the comments tonight. So, I'd like to hear a vote. **Do we open this up for comments from our residents? All those in favor say aye.**

Mr. Terry asked can I ask a question before we do that? What are we being asked to do at the conclusion of reviewing this? Ms. Winter stated you are being asked to forward a recommendation to the City Council as to what items still need to be addressed as part of the Site Plan process. Mr. Terry stated okay, that will weigh in my decision then.

Ms. Allenspach stated the Council should be taking the public comment. Chair Plaisance stated correct. So, is there any more discussion before we take a vote?

Mr. Terry stated yes. The public will make comments tomorrow if they can attend. Is that what you're talking about when they will make comments? Or, is there another opportunity besides that? Chair Plaisance stated I believe that tomorrow is

the night that they would be able to make comments.

Mr. Terry asked could we canvass the audience as to how many would be able to attend that? Because, that could be a factor as well. Chair Plaisance stated I'd think that's redundant as to whether or not we want to listen to their comments tonight. Mr. Terry stated right, but if they were not informed and they're just finding out about this tonight, this might be their only opportunity to comment. Or, it might not if they can. Chair Plaisance stated that's true. Then you would be voting as that is your interest according to what we are doing tonight. Mr. Terry stated right, that's why I wanted, although you don't have to, but that's why I thought if we knew. Chair Plaisance stated I think we should be making a decision as to whether or not we are going to allow comments from our residents. Mr. Terry stated okay.

Chair Plaisance asked any other discussion? **All those in favor say aye: Ms. Bonin and Mr. Terry.** Chair Plaisance stated **all those opposed nay: Chair Plaisance.** Mr. Cornicelli stated I could go either way. Chair Plaisance stated that's the definitive yes. Ms. Winter asked how many yeses? Chair Plaisance stated I heard two yeses from Lorraine and from Glenn. I heard a definite maybe from Lou. I said no. I did not hear from the other members. Ms. Allenspach. I stated yes. Mr. Holmes stated I said yes, you probably didn't hear it. Chair Plaisance stated no, I did not hear you. So we have four yeses, one abstain, one naye.

Mr. Cornicelli stated one not sure. I guess my, 'I'm not sure' is what, I appreciate folks being here and I've been on that side too, commenting on issues. Is there something to be served by them spending time providing public comment to us when we're not acting on that public comment? We're just really supposed to look at this from the first perspective of the Planning Commission. So, you know, I don't know if there's a benefit. I don't want people to think it's a waste of time to come to a meeting, because it's not. But is there a benefit to taking public comment at this time because this isn't, we're not ruling on anything? We're just kind of giving our initial thoughts. Chair Plaisance stated that's true. Mr. Cornicelli stated and I suspect there's going to be ample opportunity for formal public comment both to the City Council and also the Commission if it gets that far. Right?

Ms. Bonin stated I just think if people came expecting to have a chance to say something, even if we limit the length of time that we'll take comments, I think we should take at least a few comments.

Chair Plaisance stated as to the vote, we have decided to take comments from the residents tonight. So if you wish to make a comment, you certainly may. You can come up to the podium. Please speak your name and your address and state your concerns.

Andrew Mycka, 23554 Goodhue Street NE, stated I think a lot of the problem is we just have questions. We feel like we're in the dark on this. This just kind of got thrown on us. We didn't know that this was going there. All of a sudden, we just found out about this. And it was like, I live directly south. I own a whole large section of the watershed that is directly south of this. We don't know what the chemicals are. I have children. You know, we have concerns. There's a reason we came up here and I appreciate you letting us speak. You know, like, thank you.

But, we have a lot of questions. So, if you could just let us ask a few questions, that would be fantastic. Because, I know there's a handful of people here that really have a little bit to ask. That's all I've got to say. Thank you.

Dennis Anlauf, 590 Alaska Loop, Cambridge, Minnesota, stated I'm one of the owners of Mille Lacs Oil Company and we have the convenience store and the liquor store next door. The only comment I have is if you guys approve this, just require them to buy their fuel at our store. That's all I ask. I know how it works when there's property with certain zonings and if they meet the requirements, it's difficult for neighbors, including business neighbors, to get their opinions to sway. But, as far as we're concerned, I see both sides, the neighbors and I see the business next to us. We always hoped that whatever came next door would help to support our business and I think there's certainly a possibility of that by those guys being there. But, I also know that all these neighbors probably are customers of ours. So, I see their opinions too. So I am basically staying out of it. Thank you. Chair Plaisance stated thank you. Anyone else?

Dave Landes, 1747 237th Avenue NE, stated my wife Sherry is here. We are, if you move your slide a little bit, I'll show you how close we are to it. Ms. Winter stated you are right here. Mr. Landes stated we are directly adjacent to the east of this proposal. So, we're about as close as you can get to being affected by it. I'll keep it brief because apparently, there's going to be adequate time and I'm hoping. I'm not really clear though on how much time residents are going to have in this process. I feel a little evasiveness about it. When I hear there's no public opinion necessary to make the decision, that's frightening frankly. But, hopefully you can be clear on how much time the public will have to air their opinions on this.

Mr. Landes stated one of the things that I would think you would want to, concerned with, if this type of a facility goes in, to me it's not only the death knell to the neighborhood for our property values and desirability to be in this area, but it even sets a tone for the kind of business that would want to be adjacent to this property. If you focus it on where they are now, on the type of appearance and all the other things that are easily seen, it's quite apparent that, and safe to say that, no one would choose to be near that. Even with a business, a normal business, that would probably not be alarmed at, is not going to choose to go next to that. So you are talking about setting the tone for what comes to East Bethel.

Mr. Landes stated Mr. Davis, last night, said these folks had been turned down by a number of other communities. Davis stated that's not correct. Mr. Landes stated that's what you said last night. Davis stated no I did not. I said that they had been rumored to have been turned down. Mr. Landes stated you didn't call it a rumor last night, respectfully sir. Davis stated no and you can check the record.

Mr. Landes stated you said that you assume it was because of zoning issues. You assumed. Out of a number of communities, is it to be assumed that it was only because of zoning? I'll put my money on it that it was something more than zoning. I'll tell you that. But, anyway, I'm just commenting. So you can take that for what it's worth.

Mr. Landes stated so what my point is, is that is this what East Bethel wants for the

type of appearance and the type of businesses that we want? You're setting a, something in motion that is bigger than just this, I feel. So, can you tell the folks how much time or what are venues available for comment in this process? Not just tonight but going forward?

Chair Plaisance stated well we opened it up tonight for your comments and then tomorrow night there is a City Council Special Meeting for this where you can come in, I believe, where they can make comments and concerns. Mr. Landes asked that's what it's meant for? Chair Plaisance answered yes. And, I think it still has some more processes even before that. Or, is that it? Ms. Winter stated the Roads Commission has had an opportunity to look at this. The Economic Development Authority did look at this last night. The Planning Commission, it's before you this evening. It will go to the City Council at a Work Meeting tomorrow night. After that, then they're going through the formal Site Plan Review process. So, we don't have that full application yet. But, normally what would happen then, is it would come back before the Planning Commission at their Regular Meeting in March, which I believe is the 22nd. Again, it's a Site Plan Review. If you choose to take comments at that time, you could. But, again, it's not a public hearing. Chair Plaisance stated that is not a public hearing. Ms. Winter stated no.

Ms. Allenspach asked will there be public hearings down the road? Ms. Winter answered no. As I stated before, this is Light Industrial so from a zoning perspective, it is a permitted use in the district that it's zoned in. So, when you have a permitted use in a district it does not require a public hearing process. But, it does require a very comprehensive Site Plan Review. Ms. Allenspach stated I just wanted to make sure that everybody understood that whole process. Thank you.

Mr. Landes asked can I just address everyone? I think from what we're hearing, people better be concerned about where their opinions are going to be in this process. It sounds like this could be the type of thing that the City could take their will and not really obligated to take into the resident's wishes very seriously. So, that's all I have to say. Chair Plaisance stated thank you.

Rita Biljan, 23600 Goodhue Street NE, stated across the street from all this. So, the Site Plan Review, like the few questions that they had, is like, the pollution control and the swamp, and all that kind of stuff. That's when you guys will find out what they're actually saying they are going to put in there. Right? Right now, you only have this preliminary plan. Ms. Winter stated preliminary start of the discussion, right.

Ms. Biljan so you're not like just taking whatever. Once you hear what, how much water they're going to use, how much, how noisy it will be, how dirty it will be, all of that kind of stuff, then you'll discuss that among yourselves and decide if that's a beneficial thing for our East Bethel or not. Right? I mean that's kind of how the Site Plan thing is supposed to work? Just because it's zoned for that doesn't mean they have to accept that going in there.

Chair Plaisance stated if they are within the boundaries of the ordinances, there really isn't much we can do from our standpoint. We are here to interpret those ordinances. So, again, if they are in non-compliant with any of those ordinances, or

with the Watershed, that might be a concern. But, if they are not, then there really isn't much, I don't think, that we can do from this Commission.

Ms. Biljan stated okay, so it really doesn't matter what anybody, because that property is zoned what it is and if they meet all of those, then it goes in because nobody can stop it is what you're saying.

Mr. Terry stated that's not exactly accurate. Ms. Allenspach stated we have limited ability to control what goes on if they meet all the qualifications. Mr. Terry stated if there's a safety hazard, a public safety hazard or some issue like that, regardless of whether it's a permitted use, I don't know why any reasonable person would accept it. So, they have to demonstrate that it wouldn't be, which is what we'll be finding out at the Site meeting. Ms. Biljan stated it's a Site Plan thing, okay. Ms. Winter stated correct.

Ms. Biljan stated so then come back and find out more. So, even tomorrow night, if we came back for the City Council meeting, or whatever, we could say, 'Yeah, we don't like the sound of that.' But, that would just be our voicing our opinions and life would go on from there. Until you guys actually get that Site Plan and you understand what actually is involved, and if there's anything that's really bad, then you would say, 'no.' But if there's not, then there's not too much that can be done about it.

Mr. Terry stated for one thing, we're an advisory board to the Council. So, Council is where your comments are, have the most weight, it seems in this particular process. And, if they don't hear what your concerns are, that might not be a factor. But if they do hear your concerns, then it gives them additional consideration. Ms. Biljan stated right. Okay, all right, thanks. Chair Plaisance stated thank you.

Kathryn Morris Echols, 23615 Goodhue Street NE, stated right across from the proposed location. I have several points to mention today but I will be addressing a few more tomorrow when I'm more prepared. But, a few of my points of contention at this point is that we already have one of these companies in the area, already in the City, with numerous complaints as this gentleman said. You know, what purpose is this new location or new company coming to East Bethel serving? Or, what are they providing to East Bethel that other companies and other family businesses wouldn't be able to provide?

Ms. Morris Echols stated as the gentleman in the back of the room also mentioned, that sets the tone for what we are accepting into East Bethel and I know that a lot of our people that are here would like to see restaurants or family businesses coming into the neighborhood. It is a neighborhood. It is not an industrial area. I really, I'm concerned about watershed, chemicals leaching off.

Ms. Morris Echols stated my last point is that we talked about 30,000 to 40,000 gallons at the smaller locations and East Bethel, or Rogers and Elk River. When they combine these operations in East Bethel, how many more gallons would they be using here? And then I just also want to make sure that we heed that this would be three times as large, probably, as those Elk River and Rogers areas. So it's going to be three times as noisy, three times as much traffic, three times as much water.

And then my largest concern is the environmental impact, you know, when it comes to water and such. So, thank you. Chair Plaisance stated thank you.

Glen Thies, 2124 233rd Avenue NE, stated my wife Elaine and I have lived there since 1979. In 2004, there was a development put in. It was the Heckenlaible development. But, there's 45 homes there. I guess one of my concerns is drawing down 5.5 million gallons water would be pretty hard on that water table. We've seen it degraded since the addition, the Heckenlaible Addition, came in. There's a lot more iron in the water. I guess that's it. Who has, who will say whether 5.5 million gallons of water can be pulled out of the earth lately? Anybody?

Mr. Cornicelli stated and to clarify, that's just my quick math. 30,000 gallons times 31 times 6. I don't know if it's actually 5.5 million. Mr. Thies stated okay, close. But I mean, who can make a study and say that it's okay? I mean, I've seen a degradation for sure and when you start pulling out that amount of water, ah, it certainly won't be better water. That's my question.

Mr. Cornicelli asked so that would not be City water or sewer then? Ms. Winter indicated no. Mr. Thies stated maybe they should be. Maybe they should have City sewer also. There's one right, one mile north, over in the trailer park on the other side of the road, 65. Thanks. Chair Plaisance stated thank you.

Mr. Landes stated one more point to bring up. Last night, Mr. Davis mentioned the hours of operation. Could you tell the folks that please? Ms. Winter stated they would be running two different shifts. For their CST Distribution, it would be a 7 a.m. to 3:30 p.m. And then their second shift would be 4 p.m. to 2:30 a.m. The office hours are 8 to 5 for their CST Transportation. The office hours are 7 a.m. to 4 p.m. And the driver mechanic and warehouse hours, some drivers would start leaving as early as 4:30 a.m. depending on delivery and as late as 8 a.m. They come back anywhere between 2 to 9 p.m.

Mr. Landes asked can you restate that last (*inaudible, off mic*). Ms. Winter stated for the CST Transportation? The Drivers? *Mr. Landes off mic comment inaudible*. Ms. Winter stated drivers start leaving as early as 4:30 a.m. depending on delivery times and as late as 8 a.m. They would come back anywhere between 2 to 9 p.m.

Mr. Terry stated it's 4:30 to 2:30 a.m. Ms. Winter stated yes, correct. Mr. Landes stated make no mistake, this isn't an industrial area. This is a residential area. 2:30 a.m. is industrial activity. Keep that in mind. Ms. Winter stated that's all contained within interior. Ms. Bonin stated no it isn't because they've got to go on the street.

Mike Biljan, 23600 Goodhue Street NE, stated I'd like to know what happens to the 30,000 gallons of water daily. Where does it go when they're done with it? And, I just don't understand where they're going to dump that or put that back in the ground. Is that going to seep into our water? I mean, where does it go? Nobody's stated that yet. Chair Plaisance stated thank you.

Matt Echols, 23615 Goodhue Street NE, stated they talk about screening along there. But, it also talks about stockpiles, 30,000 yards, two of them. How tall is that? I'm pretty sure 30,000 yards is going to be a lot taller than the trees or the berm that they're planning on building. About the water issue too, doesn't East

Bethel have water up to 237th? Or no? Is it farther down? Does anybody know? I mean, can they tie onto the City water instead of pulling out of the wells? Ms. Winter stated it's too far away at this time. Echols stated okay, because that would be a good thing for East Bethel to be selling their water, I guess. Truck traffic is an issue too and I guess those are my two points.

Chair Plaisance stated thank you. Would anyone else like to speak on this issue? Very well, I will close the comment section.

City Administrator Jack Davis stated Randy, if I could add one more thing. Everybody that's here, there are three City Council persons in the audience tonight but I would encourage you to come back to the meeting tomorrow night when all five City Councilmembers will be present to voice your concerns. Everybody wants to hear these issues. Everything you've mentioned tonight are things we discussed last night. We're looking for those answers also. So, please come to the meeting tomorrow night and you'll be given every opportunity to speak on what your thoughts and concerns are on this matter and we'll have some representatives from the company there too so we can get some of the answers that we don't have. The others will have to come as part of this whole Site Plan Review process. Thank you. Chair Plaisance stated thanks Jack. Further discussion?

Mr. Holmes stated I'd just like to say that I appreciate the comments. I've always wanted people in East Bethel to give comments no matter what, whether it was open meeting or not. But our duty right now, I believe, as a preliminary type item is to allow them to see if it's going to work. And, we'd have to have pretty good reason for it not to work to turn them down, I think, because it is zoned properly. If it wasn't zoned properly, then that's a totally different story. And, I'm sure there's going to be, I can just see a lot of problems with it. But until they know. I mean we don't even have our answers ourselves yet. So until we know what the problems are with this location, I don't see any way we could turn it down.

Mr. Holmes stated as far as the traffic goes, if this doesn't go in there and say a shopping center goes in there, there's going to be just as much traffic. Ms. Allenspach stated or worse. Mr. Holmes stated so it's really a non-issue if you ask me. Ms. Bonin stated right but that would be car traffic. Mr. Holmes stated yeah, it would be different traffic but you'd probably have a lot more cars than 29 semis. So, that's just a comment. Not saying I'd prefer to have it there but just something that I think 'legally' we have to do, I would say.

Mr. Cornicelli stated but also there's a list of stuff. We can advance it or state concerns. I mean, I can tick down a list that I had or we can, however you want to do it.

Mr. Terry stated regarding the zoning, we sometimes have looked at changing zoning to accommodate something. I don't know that we've ever looked at changing zoning to not accommodate something. But maybe it's not so smart to have the zoning we have if something with that kind of impact is right up against a residential area. I mean, it's against the highway, which is fine. But I could see, although I'm still really concerned about the groundwater issue and noise and smell, but if it were not, if there was a buffer between it and a residential zone, I'd have

less concerns than when it's right up against one and a church, for that matter.

Ms. Bonin stated I would say if it were up by Cemstone rather than down at this end of that big property, that would be a whole different story as far as I am concerned. Ms. Winter stated that property wasn't for sale. Ms. Bonin stated no that's, I understand that.

Mr. Terry stated this is also not too far from a very tremendous resource of the Cedar Creek Ecosystem Science Reserve. A very sensitive natural area that has been undisturbed, for the most part, from human activity. So that particularly, in regard to impact on the water, is a concern that I have.

Chair Plaisance asked Lou, did you want to address your comments? Mr. Cornicelli stated no I just said the tick list that I had is certainly water use, you know, seasonal quantity? What type of dyes are they using? Where is the wastewater going? I know there's retention ponds planned but what's the, what's settling in those ponds? We don't even know what kind of dyes these are. Certainly, the road access, whether it's a Wal-Mart or this, you can't put two semis making a left turn right at Cooper's Corner. So there's road issues. What's the runoff, the base runoff's going to be? What's the permitting for that much water? What's the noise? And, like Glenn said, get the, I get our constraints if it's legally zoned right now. There's only so much you can say. But, I think there's a tick list of things that would need to be addressed. Principally water. I'm not a hydrologist and there's all sorts of permitting for that but it's something that's a pretty significant amount of water.

Mr. Holmes stated well if it's allowed, I think unless we have something that's absolutely against it, I think we've got to allow the preliminary synopsis of the problem and then when it comes back, we can do whatever we want. Mr. Cornicelli stated I agree. Yeah, I think we would all agree with that. Mr. Holmes stated I think we owe it to those people also. I mean, they want to build here and obviously, we're looking for companies here to build in East Bethel. Now when we find out what the problems are, that there's, it causes too much damage, obviously we're not going to vote for it, I wouldn't think. So, until we find that out, because we really have no idea what it is right now. Mr. Cornicelli stated right. Ms. Allenspach stated a lot of unknowns right now.

Mr. Terry asked on the second shift, do you know if that's a noise generating activity, they do at that time? Ms. Winter answered I don't. Mr. Terry stated because that would be impossible nearly to tolerate living next to somebody that's making loud noises up to 2:30 in the morning. Ms. Winter stated I know that in our existing Classic Commercial Park Aggressive Hydraulics does run two separate shifts inside their buildings. They're not right next to residential but they're pretty close to some residential. I have never gotten any complaints about that. Mr. Holmes stated but that's in, inside facility, right? Ms. Winter stated correct. Mr. Terry stated they're doing their dyeing outside.

Ms. Allenspach stated it would also be inside, correct? Didn't you say their second shift would also be inside? Aren't they the dyeing and bagging people? Ms. Winter stated I guess I'm not sure. Ms. Allenspach stated but still a concern. Mr. Cornicelli stated add it to the list. Ms. Allenspach stated it's quite the list. City

Council has their work cut out for them.

Chair Plaisance asked any more comments or concerns?

An unidentified gentleman in the audience asked can I make one more comment? Chair Plaisance stated I'm sorry but we've closed the public comment section. The unidentified gentleman stated I'm concerned about the noise thing. Ms. Allenspach stated yes so are we.

Chair Plaisance stated it's noted. We are trying to address all these issues and I am sorry, but we do have to have some kind of a decorum in order to move the meeting along. Again, if you have some more comments, please come tomorrow night and come to the meeting then. So, thank you.

Chair Plaisance stated I will close this particular CST Distribution Concept Plan, 6.0 discussion and move on to 7.0.

Submitted by:
Carla Wirth
TimeSaver Off Site Secretarial Inc.

EAST BETHEL PLANNING COMMISSION MEETING
March 22, 2016

The Planning Commission met for a regular meeting at 7:00 pm at East Bethel City Hall.

MEMBERS PRESENT: Randy Plaisance, Chair Glenn Terry Tanner Balfany
 Eldon Holmes Lou Cornicelli

ABSENT: Lorraine Bonin
 Sherry Allenspach

ALSO PRESENT: Colleen Winter, Community Development Director
 Tim Harrington, City Council Liaison

1. Call to Order Chair Plaisance called the meeting to order at 7:00 pm.

2. Adopt Agenda **Mr. Holmes moved and Mr. Balfany seconded to approve the agenda as presented. Motion carried.**

3. Approval of 2/23/16 Minutes Mr. Terry questioned why on page 7 all of the discussion made by the Planning Commission on the item of the proposed CST business was summarized in the sentence “The Commission consensus was that visual impact, traffic issues, and environmental issues all need to be addressed.” Mr. Terry stated that there were a lot of serious points and discussion raised and that he didn’t know why that was omitted and so tersely abbreviated. Ms. Winter offered to go back and review the tape and add the comments back into the minutes. She reminded the commission that the format of the meeting minutes are no longer verbatim minutes, but are summary minutes. Mr. Terry noted all audience member comments were verbatim. Ms. Winter reiterated that Commission comments could be added to the minutes. Mr. Terry believes that that is important, as there were issues raised that were not brought up by the public and that that is a very impactful design and issue that should be looked at. Ms. Winter suggested the Chair table approval of these minutes until the April meeting when a revised set of minutes that reflects a more verbatim style can be presented for approval. Chair Plaisance asked if there were further changes to the minutes. Mr. Holmes said that the minutes could be passed, except that section on CST. Chair Plaisance stated he thought that the minutes needed to be approved as completed minutes, Mr. Holmes stated that was not the case. Chair Plaisance stated he thought the minutes should be tabled until the next Planning Commission meeting and recommended same. Mr. Terry made a recommendation to not reprint the whole of the minutes for the next packet, but only the amended portion.

4. Final Plat for Sauter’s Commercial Park 2nd Addition Final Plat – Sauter’s Commercial Park 2nd Addition
 Property Owner: T & G Land Inc./Tom Sauter
 Address: 1052 189th St. NE, East Bethel, MN 55011
 PIN: 32-33-23-22-0002
 Zoning: Light Industrial

Requested Action: Final Plat approval

Background Information:

At the February 23, 2016 Planning Commission meeting a Preliminary Plat for Sauter Commercial Park 2nd Addition was approved. Before you is the Final Plat of Sauter Commercial Park 2nd Addition. At this time Mr. Sauter is proposing to plat only two lots and an Outlot.

Comments:

1. All required documents as outlined in our Subdivision Ordinance Chapter 66 have been submitted and review and comments have been given per our City Engineer, all appropriate changes were made on the Preliminary Plat to align with the future Service Road.
2. The Applicant submitted a Joint Application form for Activities affecting Water resources and there will be no impact to existing wetlands. A wetland delineation was completed.
3. Lot 1, Block 2 will remain a single family residence at this time.
4. Mr. Sauter has agreed to dedicate the right of way for the City of East Bethel to complete the extension of a Service Road (Buchanan St and 189th).
5. A Developer's agreement will be drafted and approved by the City Council at the same time as the Final Plat.

Mr. Balfany moved and Mr. Holmes seconded to approve Final Plat Sauter's Commercial Park 2nd Addition as presented. Motion carried.

5. CST Update

Chair Plaisance noted this is an information only item and that no action is required.

Background Information:

Staff and City Council have answered a number of e-mails, responded to calls and have met with 2 individuals relating to the proposed CST location.

Staff met with CST on March 8, 2016 regarding the concept plan and site plan application submitted to the City. Staff provided CST with the following information relating to the Site Plan

Review Process:

- CST was advised that their timeline for the project was overly optimistic and was given a revised timeline. The first actionable item – Site Plan approval will come before the Planning Commission at the regular meeting scheduled on April 26, 2016.
- CST was advised of the petition opposing their location at 23805 Hwy. 65. The prospect of a community meeting to allow CST to address concerns of the project was discussed. CST was advised that the City would have no role in the meeting other than to offer a location for the gathering.
- CST provided a site plan application to the City on March 1, 2016 and additional items that need to be addressed included environmental concerns, wetland delineation, signage, traffic, and visual appearance.
- CST was advised that the burden of proof relating to noise, dust, particulate

matter and other requirements contained in Code must be provided to address the conditions set forth in City Ordinance and CST must clearly demonstrate to the City that these issues do not have any impact beyond their proposed site.

- CST was advised that they must obtain all required permits from the DNR, MPCA, Anoka County Highway Department and any other regulatory or permitting agencies that have jurisdictional authority over these matters before the City could issue any permits relating to this project.

Mr. Terry asked if there was a specific number of how much ground water CST was projected to use and if that number was at their beginning stages or as the business is projected to intensify over time. Ms. Winter replied that a range was given at a different meeting, however, it was not a specific number. CST talked about usage at their beginning stages, current usage, as well as projected usage. CST is aware that they will have to work with the DNR and get appropriate permitting for water usage. There is no specific language in City Code on restricting water usage, however, the Code is clear on what permits are required from other agencies.

Mr. Cornicelli asked Mr. Holmes how this property came to be zoned light industrial across from residential zoning, as it has been an ag field for many years. Mr. Holmes said that when they went through this initially, the City needed x% of light industrial land, x% of multi-residential, x% business, etc. Mr. Cornicelli clarified that it wasn't that this space had to be zoned light industrial, but that it was chosen to fill the space. Ms. Winter stated her understanding is that when the City did the Comp Plan back in 2005-2007, one item reviewed was visioning for development in the city over a number of years. The City wanted to have continuous space designated as future light industrial to allow for businesses to locate in the future. Mr. Cornicelli noted that the road is not really set up for this type of activity. Questions regarding roads and traffic are for Anoka County to answer, rather than the City.

Chair Plaisance reminded audience members that this meeting is not an open forum, that it is an information update for the Commission. He did ensure the audience that their concerns and thoughts are important, however, this is not the meeting to be heard.

Mr. Balfany asked if CST made any comments on having a community meeting and if they are willing to move forward with a meeting. Ms. Winter said they have not, but that the owners have a willingness to show residents their current site.

Mr. Holmes reported on his 2.5 hour visit to CST's Elk River site on March 23. Chad (owner) was very congenial. When Mr. Holmes asked why CST wants to move, he was told that the business needs more space. Mr. Holmes circulated pictures he took of pallets (packaged pallets) and light wood piles. Currently, the piles are about 35' in height, however, CST prefers to keep the product piles at 15' or less, with a maximum height of 20', in order to avoid product loss to wind. CST is currently on 12.5 acres; the East Bethel site is 37-38 acres. Mr. Holmes saw maybe 6 or 7 employees, there are not a lot of

employees. Most truck traffic will be in the fall; generally 30-40 trucks per day that would start around 9:00 am. Trucks are not usually stored onsite, as the drivers take them home. Mr. Holmes was asked if they were grinding mulch while he was there. He stated that they don't grind mulch, they truck in the mulch from McGregor, from Shenandoah Forest. That is another reason why Chad would like to be on Hwy 65, because it is a lot closer. Mulch was being colored while Mr. Holmes was visiting. CST is willing to reuse some of the water it uses and is willing to hook up to the sewer system, if the City wants CST to hook up. Mr. Holmes asked specifically about fencing and safety precautions at the proposed site. Mr. Holmes' understanding is that CST is going to install a fence from the main building going south and then east back up to the northeast corner. There are no plans to fence the north side, however, if there is a problem, CST will fence the northern side. CST also plans on having 8' berms with trees. There are two proposed gates - one on the proposed main entrance and one behind the gas station. Mr. Holmes asked about there being a gate on the proposed service road site. Chad said that until a service road is installed, that location will have a gate.

Anoka County is reviewing the service road and the location further east and not behind the gas station. Mr. Cornicelli said that makes sense since realistically no matter how it is zoned, if they were talking about a Wal Mart it would be the same argument, there couldn't be more than 4 or 5 cars in the left turn lane until they're backed into the intersection. Anoka County decides locations of service roads.

Mr. Holmes continued with his report. While there he asked what type of dye is used and stated that it was literally coming out of the dye machine into the packaging and that he put his hand into it and that it left no color on his hand - it is a fast drying dye. The noise was unbelievably minimal. No smell to the product, even with the wind blowing into his face. Yes, there would be a smell if you were right next to the cedar, however, being 30' away there was no smell. Chair Plaisance stated there being a smell was one of his biggest concerns. Mr. Holmes was impressed with there being no smell. Mr. Terry asked if this operation was at a slow pace until the summer or later in the spring. Mr. Holmes said no, it's about the same pace all the time, until fall when there is heavy truck traffic going in and out to deliver the product. The mulch preparation and dying is being done longer than just the summer. Mr. Balfany's impression is that this business does most of its business throughout the winter as far as the dying, the mulching and deliveries and then it's just going out all summer. So it is building a stockpile during the winter months when it's not selling in preparation for the spring/summer season. Mr. Holmes said correct, as all the pallets in the pictures are frozen to the ground and have been sitting there since fall. However, fall is its busy time for trucking material. They do work year-round and they might work until 1:00 or 2:00 am, but this doesn't happen very often from what Chad said. Mr. Holmes reiterated that Chad was very cordial and willing to work with the City of East Bethel. Residents are encouraged to visit the Elk River site. Mr. Holmes said that if residents are really concerned, they should visit the site and talk with Chad. CST has a short window of time to move, since the changing of product

is in August. CST anticipates once the product is gone from the Elk River site, it will take about 2 days to move equipment. The packing house/bagging facility is currently a Quonset type plastic shelter heated with electric heaters. The new building will be insulated to deaden sound. Above ground storage tanks for dyes and such will be located north of the building. Again, Mr. Holmes stated Chad was very impressive to talk with and that he is willing to do what is necessary to keep residents and the City happy.

Chair Plaisance again asked audience members to not comment during this informational meeting. The last meeting was opened up for comments. It's not that the Commission is not concerned with the public's thoughts, it is. However, there are certain items the Commission has to get through and certain items on which the Commission has to be instructed. If residents want to talk with Commissioners, Chair Plaisance stated that most members would be willing to talk with them after the meeting. Right now, this item is for Commission discussion and edification, and to please keep their comments out of the current discussion.

Mr. Holmes believes he is a very good judge of character and he doesn't believe Chad is trying to steer the City of East Bethel down the road. A concern Mr. Holmes has is that even though he saw 6-7 employees during his visit, he questions even if the business grows whether more employees will be hired from the East Bethel area; that is a consideration with incoming businesses. That is the only real drawback that Mr. Holmes had after visiting the site. Generally, people that want to hide something don't let you walk around and do whatever you want to do.

One thing that can't be seen from a visit and one of Mr. Terry's biggest concerns is groundwater. Even if everything else is above board and not all that impactful, if the groundwater is depleted, what is East Bethel going to do? How does the City have insurances that that won't be the case? Ms. Winter stated that is why the City relies on other agencies that have review authority over this, to be responsible to monitor and issue permits. Mr. Cornicelli stated that the City of Elk River must have some records of usage in order to calculate if the business is 20% larger, than the usage could be 20% more or minus, and that that current information would be useful. Ms. Winter said that is currently part of the whole application process they have to go through when they submit their applications for those permits. Due diligence is done through applications permits, etc. CST does fall under a different category as far as the DNR is concerned, so as part of their site plan, those applications are being put together now.

The City had discussion with CST regarding the original very aggressive timeline. The City put together a more realistic timeline given the need for information and steps involved. CST knows that the first potentially actionable item may be the end of April and that the timeline is expanded out.

Mr. Cornicelli asked Ms. Winter when would be key spots during that timeline for residents will have the opportunity to speak out in a public forum.

Depending on whether or not CST has to go through platting, which is not clear at this time due to the whole service road discussion, there could be an opportunity to have a public hearing. However, that public hearing is to deal with the plat only and not anything related to the project itself. City staff and City Council members, have been contacted by residents concerning CST. Ms. Winter said she believes ample opportunities have been provided for audience members to speak. From the Planning Commission's standpoint, if it's a planning site review, that is not a public hearing. That would not be an opportunity for public input, unless the Chair opened it up for public input. The best time for the public to be heard is during the public forum portion of City Council meetings. Mr. Balfany thanked Ms. Winter for the clarification and did tell audience members that they should be heard. Mr. Harrington reminded the audience that the Town Hall Meeting will be April 19th at 6:00 pm and that that will be another opportunity to be heard.

Chair Plaisance stated it would be good to see the reports from the various permitting agencies. Mr. Cornicelli ask that tangible information be given to the commissioners. Ms. Winter offered to provide a mid-line report throughout the timeline. Because this is a highly controversial issue that has an enduring impact on the community, Mr. Cornicelli believes any additional information will help the Commission to make the best recommendation to the City Council as possible. Mr. Holmes asked if they could have a current water use report from the regulatory authority. Mr. Cornicelli stated as previously discussed, it is important to know what the mitigation plan is for the wastewaters, the dye oil base vs. the water base, how much water are they truly going to be using, what is the real traffic flow, etc. Mr. Cornicelli noted that as Mr. Davis stated, hands are limited in light industrial space whether it is a fit or not, but these are reasonable questions to be asked before this is considered by the Commission.

Mr. Holmes did ask Chad how often trucks would be going from one pallet storage place to the other and he said 6-8 time a day. Chair Plaisance said that was his concern and one reason was because of the service road and if another company were to building behind CST, would that pose a traffic hazard. Mr. Cornicelli said he believes the current intersection at Hwy 65 and 237th is not set up for more than residential traffic at this time. Mr. Holmes is not thrilled about a service road anywhere on the property, but that he knows that that is what the City wants to do and that is what the City has planned. Chair Plaisance asked the City or the County. Mr. Holmes said the County wants it, but that the City also has wanted it for quite a while. Ms. Winter said it's part of the whole service road plan as you continue to move north. Any time you have development, you have to have a plan that indicates where traffic patterns are and where it makes sense to have service roads. With all the work East Bethel is doing with MnDOT, as well as with Anoka County, that is a huge priority item for them. MnDOT and Anoka County are working to limit access onto Hwy 65 and they have sort of charged East Bethel with the task of looking at service roads as another option for getting traffic to and from places. Chair Plaisance asked if CST has a plan for moving product across the road for when it does become a service road. Mr. Holmes said that CST puts

up stop signs for their drivers to go across. There is still a problem; it will be a forklift driver against a vehicle. That is why Mr. Holmes does not like having the service road there and would rather see it in the front, however, he does understand why others do not want it in front. He doesn't believe that can be located on the eastern side because of groundwater. Ms. Winter also noted that there are wetlands to deal with on the eastern side as well. Mr. Balfany would like to hear from MnDOT and Anoka County on the Hwy 65 situation. That in itself could put a hitch in everything, unless they are going to change the roads.

Mr. Holmes suggested each commissioner go visit the Elk River site. He went over there with a negative attitude, but it was much better than when he first went there.

6. Home
Occupation
Review

Background Information:

Home Occupations continue to be an enforcement problem for the City. Currently we have six complaints about home occupations. Those complaints range from operating without a permit, to exceeding total number of vehicles, junk and debris. Automobile repair seems to be the biggest problem, and our Home occupation ordinance does not do a good job of addressing this issue.

Staff is requesting that the Planning Commission consider the following:

- It is not uncommon for metro area cities to list occupations such as body shops, landscaping businesses, and motor vehicle repairs or sales as prohibited home occupations.
- Many cities do not allow any person, other than the property owner, whom must reside on the premise, to be engaged in the home occupation.
- In East Bethel, uses such as motor vehicle repair are allowed in the Highway Business District and Light Industrial district. A question to consider, should the city allow uses permitted in the B3 and I1 districts as home occupations?
- Because the existing ordinance does not specifically prohibit motor vehicle repairs or small engine repairs these businesses could be considered home occupations if they meet all of the other requirements of the ordinance.

This topic has been brought before the Planning Commission as a discussion item at past meetings. The Planning Commission discussed this back in 2011 and those minutes are attached for your review and again in 2014 there was a lengthy discussion and public hearing regarding this issue. At that time, the Planning Commission recommended only one minor change to the ordinance and that was approved by the City Council. Since that time the City has implemented a new code compliance system that allows us to better track complaints and we are working on making sure that all issued IUP's are in compliance. Automobile and small engine repair continue to still be an issue for the City and staff does not feel that these businesses should be allowed as permitted Home occupations.

The City currently has six active complaints. Complaints usually come from neighbors regarding the number of vehicles on site. When individuals are asked about the vehicles on site, they usually say they are fixing relatives'

vehicles. By disallowing these types of businesses, those individuals claiming to be fixing relatives' vehicles will need to meet City requirements, i.e. five vehicle maximum all being licensed. It will also help the City to combat junk vehicles on properties. The same thing pertains to small engine. Trash heaps in front of and behind houses are covered under the junk vehicle and debris ordinance. Chair Plaisance is concerned that a blanket statement that prohibits working on cars or small engine will affect those individuals that truly are working on their own car or small engine.

Mr. Holmes' biggest problem, which may be due to shortage of staff, is that nothing seems to get done about his complaints and that the complaints get ignored. He believes the City has an ordinance for no reason. He has asked why staff isn't sent out to tell the people what is wrong and what needs to be changed and has been told people are being sent out there. To him the ordinance is non-useful. He has talked with people at the City and was told that someone has to say something about the violation before the City can do something about it. Mr. Holmes believes any city official should be able to stop and tell someone that they are not in compliance with an ordinance. Obviously, the City Inspector has to be driving by these homes; it wouldn't take much time to drop off a sheet listing the violation and a deadline for adherence to the ordinance. Another commissioner stated that by handing out a violation it now becomes an enforcement issue.

Ms. Winter interjected that the City has implemented a new code compliance system which has been very useful. Many more issues are being addressed in a much timelier way. A first letter, then if need be a second letter is sent. If the violation is still not resolved, it gets turned over to the City attorney or a fine is incurred, pending on the violation. However, fining someone doesn't always stop the violation. 98% of the time a clean-up is done after receipt of a second notice. There are frequent violators that continue to be a problem. To Mr. Holmes' point, Ms. Winter does believe the Building Inspector and Building Official do a good job when they are out. Their primary roles are Building Inspector and Building Official and those duties, not necessarily dealing with code-compliance. If they see a blatant violation, they will say something. Most violations against code are complaint driven due to the large geographical area of East Bethel. And, no, there isn't an employee to just deal with code complaints. Non-compliant septic systems makes up a big amount of code violations. However, with the sale of homes and other things that are done, there have been a lot of code compliances made. Again, Ms. Winter believes this is due to the new system in place.

Mr. Balfany agreed that enforcement is key and that understaffing is an underlying issue. Mr. Balfany does not agree that complaints are not being addressed.

Mr. Cornicelli agrees that the Commission does have to address what is allowed in regard to home businesses fixing cars and having hazardous waste permits. Unless the City is going to address the big issue of hazardous waste, such businesses shouldn't be allowed. Based on the packet information, it

appears most cities don't allow it. **Consensus of the Commission was to have City staff bring back revised Zoning Code language for review.** Mr. Balfany wants to be careful of blanket statements and wants to use correct verbiage.

7. Lowest Floor
Elevation
review

This is an informational item.

Background Information:

The City of East Bethel has had numerous discussions regarding this topic before and the request has been made to bring it back to the Planning Commission to consider changing the ordinance.

Current City ordinance in the Shoreland Management Areas:

New Construction and additions need to be located three feet above: Whichever is greater of the regulatory floodplain, highest known water level (mottled soils), or ordinary high water level.

These same rules are applied city wide per engineering standards.

Currently the Shoreland Management ordinance, Floodplain ordinance, Subdivision Ordinance and engineering manual all deal with lowest floor elevation differently. In order to be consistent and adopt the same standards, proposed changes to the minimum lowest floor elevation will be presented at the next Planning Commission meeting and will include better definitions, exceptions and decreased standards for existing structures.

Changing the lowest floor elevation provides an opportunity for home additions, accessory buildings, etc. to be built without having to follow a very stringent standard when it comes to mottled soils. There is argument with new home construction that 3' above mottle soil is excessive. Thus, City staff has asked the Commission to review this and consider 2' above. In most cases, rural residential new construction has already been engineered and are exempt, so if there is an existing sub-division that has been approved, those lowest floor elevations are determined as part of engineering requirements for that sub-division.

Mr. Holmes asked if this involves any floodplain items. Ms. Winter said it does where there are floodplains. City staff received a number of phone calls when the new floodplain maps were adopted, it changed many homes from being out of a floodplain to being in a floodplain. The problem is in most cases those base flood elevations haven't been determined. Fortunately, in the areas where there are engineered sub-divisions, floor elevation information can be given; this process is time consuming for City staff.

East Bethel's current lowest floor elevation is above 3' and the City would like it changed to 2'.

Mr. Holmes noted that an engineer usually determines the correct level. He doesn't want to see the costs for this passed on to residents, nor to the City.

Soil borings are required when building new home, so that cost is paid by the developer. It's those soil borings that determine the elevation and where the house could be built. Ms. Winter suggested City staff do more research on this item and bring back language to the next meeting. Changing the level from 3' to 2' will help with the elevation for a house, but not if the water level rises and floods can the homeowner then come back to the City because it changed the water level to 2'? Ms. Winter stated that is why the current language states "Whichever is greater of the regulatory floodplain, highest known water level (mottled soils), or ordinary high water level." Mr. Holmes wondered if changing it by 1' will affect much. Is it worth changing?

Chair Plaisance referred to the packet information for on lowest floor elevations for surrounding cities. Most are 3', with the exception of Oak Grove, which is 1'. Andover has 3' above the seasonal high water mark or 2' above the designated 100 year flood elevation, whichever is larger. Is East Bethel thinking to have a flat out 2' or a graduated difference depending upon the 100 year floodplain or whatever may be the case? Ms. Winter said you would still need to know the base flood elevation. She again requested to bring this item back to the Commission after talking with the City engineer. Chair Plaisance requested information on how many residents could be affected by making this change. Mr. Holmes noted that East Bethel probably has more groundwater than any of the surrounding cities, with the exception of St. Francis, so that too will need to be taken into consideration.

8. City Council Report

Mr. Harrington, City Council liaison reported:

- Council approved the variance setback for 19308 East Front Blvd. by Coon Lake.
- Council approved EDA request to participate in the 2016 MnCAR Expo (Minnesota Commercial Association of Realtors).
- Town Hall meeting is scheduled for April 19. Agenda items will most likely include CST, and Superstreet.
- Council is in closed session for negotiation of a new public works contract.
- Recycling day is April 23 from 8-noon at the ice arena.
- Pet clinic is April 2 from 9-noon at the ice arena.

9. Other Business

None

10. Adjourn

Mr. Balfany moved and Mr. Cornicelli seconded to adjourn at 8:11 pm. Motion carried.

Respectfully submitted,

Gail Gessner, Recording Secretary
Submitted 3/25/16



City of East Bethel Planning Commission Agenda Information

Date:

April 26, 2016

Agenda Item Number:

Item 3.0

Agenda Item:

Owner: Rimma Medelberg
PID # - 223323110006
Zoning – Rural Residential (RR)
20381 East Bethel Blvd.

Ms. Medelberg is interested in subdividing her property into two separate parcels for the purpose of selling. One property would include the existing residential home and two and half acres. The other property would be the remaining balance of the land which is almost 27 acres. If you will recall this property was brought before the Planning Commission last year and was recommended for approval to the City Council. At that time the property division was under the Metes and bounds rules and the City Council rejected the application due to lack of 300 feet of frontage for both lots along East Bethel Blvd. Ms. Medelberg is now interested in going through the subdivision process and before you is the Concept plan.

Requested Action:

Recommend Approval of the Concept Plan and call for the public hearing.

Attachments:

- 1. Subdivision Plat
- 2. Location map

Planning Commission Action

Motion by: _____

Second by: _____

Vote Yes: _____

Vote No: _____

No Action Required: _____

MINOR SUBDIVISION

~for~ RIMMA MEDELBERG

~of~ 20381 EAST BETHEL BLVD
EAST BETHEL, MN 55011

EXISTING PROPERTY DESCRIPTION:

That part of Government Lot 3, Section 22, Township 33, Range 23, Anoka County, Minnesota, lying south of the north 469.50 feet thereof, and lying north of the south 412.00 feet thereof, and lying east of the centerline of County State Aid Highway No. 15, said centerline being described as follows:

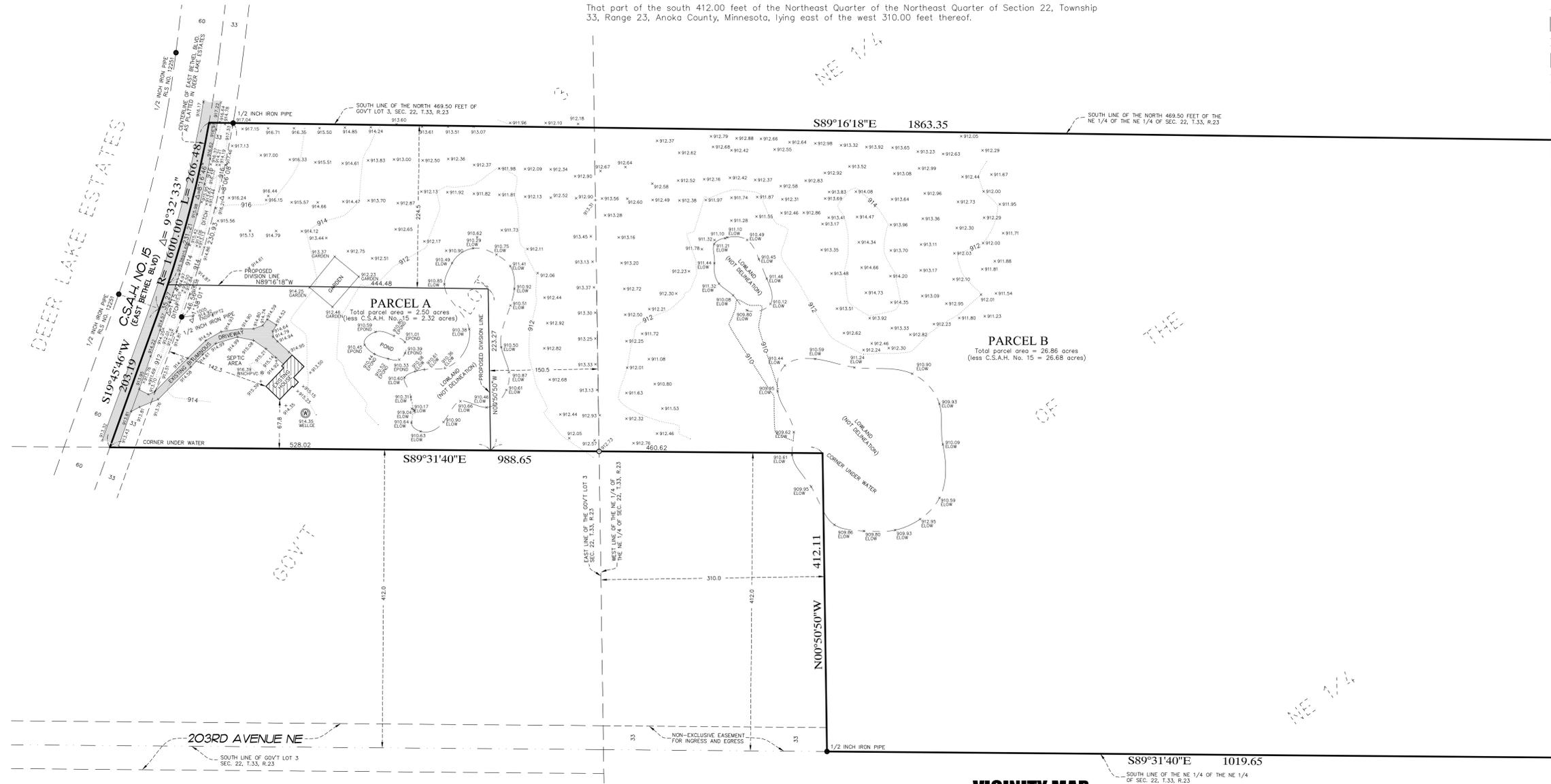
Commencing at the intersection of the north line of said Government Lot 3 and the centerline of County State Aid Highway No. 15, as now laid out and traveled, said point of intersection being 467.52 feet west of the northeast corner of said Government Lot 3; thence South 6 degrees 27 minutes 23 seconds West (assumed bearing of the north line of said Lot 3 is West) along said centerline a distance of 567.38 feet; thence South 18 degrees 59 minutes 35 seconds West along said centerline to the south line of said Government Lot 3.

AND

That part of the Northeast Quarter of the Northeast Quarter of Section 22, Township 33, Range 23, Anoka County, Minnesota, lying south of the north 469.50 feet thereof and lying north of the south 412.00 feet thereof.

AND

That part of the south 412.00 feet of the Northeast Quarter of the Northeast Quarter of Section 22, Township 33, Range 23, Anoka County, Minnesota, lying east of the west 310.00 feet thereof.



NOTES

- Field survey was completed by E.G. Rud and Sons, Inc. on 8/19/2014.
- Property description per Quit Claim Deed recorded 2/13/2009 as Document No. 2005507.001
- Bearings shown are on the Anoka County Coordinate System.
- This survey was prepared without the benefit of title work. Additional easements, restrictions and/or encumbrances may exist other than those shown hereon. Survey subject to revision upon receipt of a current title commitment or an attorney's title opinion.
- C.S.A.H. No. 15 is shown per plot of DEER LAKE ESTATES.
- BENCHMARK: MNDOT Station: DENN MNDT. Elevation = 899.21 (NAVD 88)

LEGEND

- DENOTES IRON MONUMENT FOUND AS LABELED
- DENOTES IRON MONUMENT SET, MARKED RLS 41578
- x 992.36 DENOTES EXISTING SPOT ELEVATION
- ⊕ DENOTES WELL
- DENOTES EXISTING CONTOURS
- ▬ DENOTES BITUMINOUS SURFACE

VICINITY MAP

PART OF SEC. 22, TWP. 33, RNG. 23

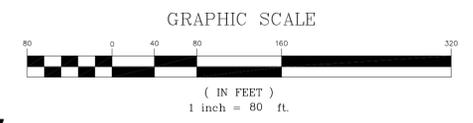


ANOKA COUNTY, MINNESOTA
(NO SCALE)

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Registered Land Surveyor under the laws of the State of Minnesota.

PRELIMINARY
JASON E. RUD

Date: 4/19/2016 License No. 41578



DRAWN BY: BAB	JOB NO: 14482PP	DATE: 8/20/14	
CHECK BY: JER	SCANNED		
1	8/22/14	CLIENT COMMENTS	BAB
2	8/28/14	ADDL TOPO/MONUMENT DIV. LINE	BAB
3	4/16/15	REVISE PROPOSED DIVISION LINE	BAB
4	4/19/16	REVISE PROPOSED DIVISION LINE	JEN
NO.	DATE	DESCRIPTION	BY

E.G. RUD & SONS, INC.
EST. 1977 Professional Land Surveyors
6776 Lake Drive NE, Suite 110
Lino Lakes, MN 55014
Tel. (651) 361-8200 Fax (651) 361-8701



City of East Bethel Planning Commission Agenda Information

Date:

April 26, 2016

Agenda Item Number:

Item 5.0

Agenda Item:

CST Site Plan Application

Requested Action:

Review and Comment relating to the CST Site Plan Application

Background Information:

In January 2016, City Staff met with Chad and Megan Toft, CST Companies, LLC and their Real Estate representative Dan Friedner (Colliers International) to express their interest in relocating and consolidating their business operations in East Bethel, Minnesota on the Mike Wyatt property, 23805 Highway 65 NE in an area zoned Light Industrial. Based on the information provided by CST the business fell under the following categories in the Light Industrial District:

- Office
- Warehouse and Distribution
- Manufacturing

Based on that initial meeting, City Staff provided CST an application for a Site Plan Review and outlined the pertinent ordinances contained within our Zoning code that would be applicable to the Site Plan Review process. Further Staff advised CST that they cannot generate noise, odor, vibration, or other discharge discernable from areas outside the parcel on which the use is located.

Attached is the complete Site Plan submission and City Staff has deemed it a complete submission that has met the minimum requirements as outlined in the Site Plan Application under Appendix A, Section 4-12. Upon receipt of an application that contains all required information, city staff shall schedule the matter for review by the planning commission. From the date the city receives the completed application, the city council must approve or deny the application within 60 days. The city may extend the 60-day period by providing written notice of the extension to the applicant before the end of the initial 60-day period. This notification must state the reason for the extension and its anticipated length, which may not exceed 60 days.

This process began in January 2016 and there have been a number of meetings, emails and discussions between representatives from CST and City Staff. City Staff have been to both the Rogers and Elk River locations of the current CST operations. Over that time period site plan comments have been provided to CST.

Included in this packet are submissions including the formal site plan and narrative. The major points of this material are as follows:

Existing Land Use

- The site is 39.2 acres in size and is currently a farm field, with two delineated wetlands.
- Current access is through a frontage road off of 237th and turns into a private driveway
- Adjacent land uses – North – agriculture (farm fields), East- Single family residential home, West- Convenience store/gas station, liquor store and a home, South – 237th Avenue NE (CSAH 24)

Proposed Use – CST Companies, CST Distribution and CST Transportation and are proposing to build and utilize 56.85% of the property for business operations, and outdoor storage. The remaining acreage will be used for stormwater ponding, preservation of the existing wetlands, berming (screening) and drainage swales. The breakdown is as follows:

- 26,745 sq.ft. Office, warehouse and maintenance facility with room for a future 6,000 sq. ft. future addition
- 10,720 sq.ft. mulch bagging facility
- Truck parking for up to 20 trucks
- Outdoor Storage area – under 30% of the rear yard

CST Distribution is a wholesale distributor of softener salt, mulch, ice melt, firewood, washer fluid and bottled water and a contract packager of mulch. Customers include SuperAmerica, Holiday Station Stores, Menards, Cub Foods and Home Depot. They have 22 employees and the wages range from \$12/hr. (labor) to \$60k. CST Distribution has two shifts – M-F 7 am to 3:30 pm and M-TH 4 pm to 2:30 am. Exterior equipment used includes rubber tired loaders, forklifts, a truck fleet and coloring machine.

CST Transportation is a local/regional trucking company specializing in forklift mounted flatbed trucks. They have 33 employees and the wages range from \$31k to \$75k. Drivers may start as early as 4:30 am and finish by 6 pm.

Site Requirements

CST was required to address the following and must comply with city code regarding: Lighting; parking; screening; signage; building; utilities; grading, and landscaping.

Additional requirements included:

- a) Right of way dedication for a future service road
- b) Accommodations for expansion and on site growth, particularly as it relates to outside storage
- c) Dust and particulate matter control
- d) Noise impacts and mitigation process
- e) Water use and water quality issues
- f) Fire mitigation
- g) Anoka Co. Hwy. Dept. requirements
- h) Joint Application affecting waterways

Public Input – There has not been a public hearing on this project. However, there have been a number of neighbors that have been present at the Feb. 23 and March 22 Planning Commission meetings and many City Council meetings. Further comments were heard by the residents opposed to the project at the

Town Hall meeting on April 19, 2016. The neighbors have also presented a petition to the City stating their opposition to CST locating in East Bethel and at the proposed site. The owners of CST and their representatives met with the neighbors and residents on April 14, 2016. The City provided only the facility for the meeting and did not participate in the proceedings.

Site Plan Comments:

- **Screening and Security** – there is a proposed berm that extends along the length of the south property line and to the west. CST is also required to put in fencing along all other property lines. The proposed landscape materials are still under review and the trees that will be used for the screening are subject to City approval. CST will also have entrance gates on their access roads into their property. Additional berming/screening would be required along the proposed service road outside a dedicated 80’ right of way/easement.
- **Lighting** – LED downward facing lighting is proposed for the site and there will be 6 outside lights located on the office/warehouse building and 4 outside lights located on the bagging facility. The lighting intensity proposed does not illuminate beyond the boundaries of the site.
- **Parking** – meets the required allocated parking spaces for office and warehouse.
- **Truck parking** – there is proposed truck parking designated on the site plan for up to 20 trucks. These trucks are used in the operations to deliver mulch and other products. General maintenance of the trucks is done on site at the main building.
- **Landscaping, sign plans, architectural standards, and grading plans** have been reviewed and comments have been forwarded to CST.
- **Building plans** have been submitted as required. However full building plans will be submitted at the time of application for the building permits should the Site Plan Review be approved by City Council.

Other Requirement/Comments:

Wood chip (Mulch) piles – The proposed site plan indicates that there will be four mulch storage piles. Mulch is colored utilizing a machine that sits outside on a paved surface and then bagged inside a building on the site. There is no processing or grinding of trees on the site. The mulch is delivered ready to be colored. Rubber tired loaders and forklifts are used in the mulch operation. The mulch bagging operation is April 1 through mid November, depending on the weather. August is typically the slowest time and winter months are used for stocking raw material. Per the Fire code mulch piles cannot exceed 25 feet in height, 150 feet in width and 250 feet in length.

Fire suppression – In addition to the access roads required for all outdoor storage areas, CST would be required to have an approved hydrant and hose system or portable system to deal with fires. As noted, there was a fire at CST’s location in Elk River due to a malfunction of one of the loaders. CST has since put fire suppression system on the loaders that work around the mulch piles. Mulch piles in general are monitored for temperature to make sure they do not exceed a certain temperature. The City Fire Marshall completes inspections of all businesses within the City every three years and could inspect this facility on a more frequent interval.

Growth of company – CST’s projected growth, has raised concerns by Staff regarding their capacity to accommodate increased exterior storage needs on the site. CST has stated they propose to increase the efficiency of the mulch bagging operation to address this need. This, per CST, would allow for packaging

inventory at a higher rate to eliminate the need for additional exterior storage. The proposed bagging facility is designed to accommodate an additional production line if required.

The trucking side of the business also has grown and currently there are 14 truck/trailers proposed to be parked on site with the ability to expand to the maximum of 20 trucks parked at the site. Currently there are approximately 40 truck trips per day (round trip). Expansion and growth of the business would increase that number.

Hours of operation – Staff and residents have expressed concern regarding the hours of operation and its potential impact on adjoining properties. CST's operations will also extend beyond what are considered normal business hours and could create noise issues that could interfere and be a nuisance to the surrounding neighborhoods.

Hours of operation and activities that can be conducted during hours of operation may need to be addressed as conditions to a Developers Agreement should this review be approved by City Council.

Dust and Particulate matter – CST utilized the MPCA Air Emission Calculation worksheet for Fugitive Emission to determine if they were required to obtain a permit. The permitting threshold for total particulate matter (dust) is 100 tons per year. The proposed number emitted by CST is 12 tons per year per their calculations. MPCA confirmed that CST was not required to obtain an emissions permit based on information submitted to MPCA staff. CST has provided the City with a proposed dust control plan. This plan would require modifications should this project move forward.

Even though the MPCA does not require CST to obtain an emissions permit, this does not indicate that dust issues and particulate matter will be contained on site. There exists the potential for dust to affect adjoining and surrounding properties. The City Comprehensive Plan states that Light Industrial activities are limited to those that do not generate noise, odor, vibration, or other discharge discernable from other areas outside the parcel on which the use (CST) is located.

There have been three reported observances of dust issues from the CST site in Elk River. These reports claim to have witnessed excessive amounts of wind blown particulate matter emanating from that site. It is reasonable to assume that large piles of mulch could release dust material as the surface mulch dries which could be blown off-site when wind speeds become capable of transporting these types of materials. There is also concern about dust issues from internal service roads and any of the 500,000 SF pallet storage area that has an exposed surface from the proposed site in East Bethel.

Noise impacts and mitigation process – CST completed a noise assessment report for the proposed East Bethel location. The noise generated from this activity is primarily due to back up alarms on equipment and trucks, the operation of heavy equipment and the use of other equipment for movement and processing of mulch. This study concluded that the noise generated by CST's operations would be negligible.

This is an area of great concern and one that has been expressed by many residents. There are several measures that can be taken to mitigate noise including the use of broadband backup alarms that can be heard by employees but merge with background noise at a distance. There is also non-audible backup lights that can be used that meet OSHA requirements. There is also a proposed berm that will mitigate noise coming from the site.

While CST claims that back up alarms can be modified to reduce noise, the City has not received any OSHA material that indicates approval for these modifications or their impact on noise reduction. In addition, modifications would, if approved, only apply to those pieces of equipment that are dedicated to and used exclusively on site. This would not apply to trucks that transport to multiple offsite locations.

City Staff would recommend that CST's noise study be reviewed by an independent consultant to ensure that the studies assumptions are not flawed, mitigation measures would be effective, OSHA requirements relating to back-up alarms would meet compliance standards and noise measurements included all equipment utilized in the operation, including trucks, and that noise measurements included peak production times.

The City Comprehensive Plan states that Light Industrial activities are limited to those that do not generate odor discernable from other areas outside the parcel on which the use (CST) is located. While information has been provided to address this concern, Staff recommends additional supporting documentation be provided to definitively confirm CST's response.

Odors – CST has stated that their mulch piles are turned over quickly and do not have the same odor as non processed wood. CST claims that the odor of cedar is potentially the strongest smell coming from the site and is equivalent to what you would smell if you were to go to any garden center that has bulk mulch bins.

The City Comprehensive Plan states that Light Industrial activities are limited to those that do not generate odor discernable from other areas outside the parcel on which the use (CST) is located. The City has no baseline for measuring or determining the insignificance or magnitude of this concern. Observation of their existing facility over the course of their April through October operations would provide some indication as to the claim of odor as offensive by-product of CST.

Water use and water quality – CST is proposing to utilize an estimated total of 2,157,800 gallons per year and will be required to go through the MN DNR appropriation permit. Prior to constructing a well, the applicant will be required to complete a preliminary water appropriation assessment from the DNR. According to the DNR 2.1 gallons per year is roughly the equivalent of 28 residential households.

On site discharge of water used in the production process is also a major concern of this operation. Drainage and on-site runoff are designed to be collected in storm water holding ponds.

CST will be required to obtain an NPDES permit for on-site storm water ponds. City staff is still reviewing the runoff issues from the mulch piles as to negative impacts on the ground water, particularly during heavy storm events. City Staff is also evaluating CST's plan to ensure that the wetlands are not negatively impacted.

There are studies that have been done for other mulching operations in other states that claim that mulch production facilities have been found to have an impact on ground water as a result of these operations. While these studies are site specific and may not be comparable to this particular situation, this could be a concern and may require additional documentation to insure there are no problems with ground water contamination from site run-off and dyeing discharge.

Joint Application affecting waterways - CST has complied with requirements to delineate the wetlands. The delineation was reviewed by the local TEP (Technical Evaluation Panel) who concurred with the delineation.

Increased traffic and safety concerns on 237th – CST was required to submit their plans to Anoka County Highway Department (ACHD) for review and comment. 237th Ave. (County Road 24) is a County road and ACHD has responsibility and jurisdiction over this route. The ACHD has identified no significant concerns with the CST Project.

Potential Health Issues- Concerns have been expressed relating to potential airborne mold, respiratory irritants and dust issues that could originate from mulch operations. Residents have provided several articles to the City that identify this as a potential issue as related to mulch operations in other states. This material has been provided to the legal firm that represents CST. The findings provided in these studies have not been linked to CST's operation, may or may not be comparable to this particular operation but may be a concern that bears further investigation.

Right of Way/Easement Dedication

While the City can not require a dedication of a right of way/easement for a proposed City service road in this case, the City can request and encourage that this commitment be memorialized in an agreement approved by both parties. The City would be negligent if they did not secure an agreement for the right of way/easement for a future road through the CST site prior to any development. The requested dedication would be an extension of Davenport Street north of 237th Ave. and through the CST site.

At a point in the future, the service road will be needed to access the property north of the CST project. The location requested by the City is the most logical alignment in terms of access to 237th Avenue and to serve the projected future traffic patterns through the Light Industrial zoned acreage. Provision of this road is also consistent with the City's Comprehensive Plan.

CST has proposed to dedicate the right of way/easement but their conditions are unacceptable to the City.

Subject to Council approval, the City would accept the following as conditions for the dedication:

- Dedication of the easement by a mutual agreement that becomes effective immediately upon the sale of and transfer of title of the property to CST
- No sunset clause or time limits for construction of the road
- Dedication would not be conditioned on approval of other permits
- The City would honor the offer of no assessment for the road construction but CST would be required at their expense to construct the subgrade of the road and provide drainage structures and utility easements to City specifications and requirements.

The importance of the dedication at this time and under the City's terms is necessary to avoid:

- Conflicts in negotiations for the right of way/easement at that point in the future when the service is to be constructed
- Eliminate the potential of a taking by eminent domain and the potential of incurring court imposed costs for right of way/purchase

For these reasons it is imperative that the dedication should be completed prior to acceptance of the Site Plan Review Application.

Summary

While CST has submitted the information identified by the City's April 11, 2016 letter requesting additional information for the application and the application is considered complete terms of requirements of City Ordinance, there remain Staff concerns relating to the matters of noise, dust and

water discharge. The need for further review and verification of some of these materials are beyond staff's expertise and may require a consultant to review their submittals.

It is the opinion of City Staff that the resolution of the right of way for a future frontage/backage road should be resolved prior to the approval of the Site Review Plan. This Staff recommendation is based on the need for future access to the properties north of the CST site and to remain consistent with the City's Comprehensive Plan.

A petition requesting a EAW for the project was submitted by a group representing the neighborhoods along 237th Ave. The application was rejected as incomplete and the City has not been informed if a corrected request has been re-submitted. If there is a resubmission and it's determined that this project meets the thresholds of the Minnesota Environmental Quality Board, the City will be obligated to respond and any deliberations relating to consideration of the CST Site Plan Review will be suspended until this matter is resolved.

Staff is off the opinion that the issues relating to on site containment of activities generated by CST, noise, odor and particulate matter, potential for ground water contamination in regards to storm water run-off and discharge and spills of product used in the dying process and compliance with the City's Comprehensive Plan relative to service roads may require further review before a recommendation can be offered for this request.

In addition, there may be other questions raised by the Planning Commission or the City Council regarding the Site Plan Review Application. These will be addressed as they are presented.

Attachments:

- Attachment 1 - Site Plan
- Attachment 2 – CST narrative
- Attachment 3 – Air Emission calculation
- Attachment 4 – Dust Control plan
- Attachment 5 – Noise Assessment Plan
- Attachment 6 – Anoka County Highway Department Review, comment and requirements
- Attachment 7 – Review request 2-16 and response from CST Attorney

The Planning Commission has been sent all the information that City Council has received over the course of the last six weeks. This includes reports, video's and photographs from interested residents.

The Planning Commission received public comment on a concept plan for this project at their February 23, 2016 meeting and was provided an update of this matter at their March22, 2016 meeting.

Fiscal Impact:

To be determined.

Recommendation(s):

The Planning Commission may request additional time to analyze the material related to their charge of presenting a recommendation to City Council on this matter and may request additional meeting(s) and/or information necessary to conduct their review and develop a recommendation;

OR

The Planning Commission may conclude after deliberation of the matter at this meeting that there is adequate information available to act on this request.

Staff requests that the Planning Commission complete a review of the material related to the CST Site Plan Review and provide a recommendation to City Council as to the approval or denial of the CST Site Plan Review Application based on the following:

- Compliance with the requirements of the City Ordinance
- Compliance with consistency to the Comprehensive Plan
- Considerations of all the concerns of the discernable impact to offsite parcels adjoining and neighboring CST
- Other factors pertinent and required for this review

Planning Commission Action:

Motion by: _____

Second by: _____

Vote Yes: _____

Vote No: _____

No Action Required: _____

KEYED NOTES:

- 1) INSTALL BITUMINOUS PAVEMENT. REFER TO DETAIL 1/C6 FOR SECTION.
- 2) INSTALL INTEGRAL CURB/SIDEWALK, REFER TO DETAIL 2/C6.
- 3) CONSTRUCT CONCRETE ADA RAMP. REFER TO DETAIL 5/C6 FOR LAYOUT AND 1/C2 FOR SIGN DETAIL.
- 4) INSTALL 4" CONCRETE SIDEWALK. REFER TO DETAIL 3/C6.
- 5) INSTALL 8" GRAVEL SURFACE, MNDOT CL. V. COMPACTED TO 100% STANDARD PROCTOR.
- 6) INFILTRATION BASIN. REFER TO DETAIL 10/C6 FOR SECTION.
- 7) INSTALL "STOP" TRAFFIC CONTROL SIGN, SEE DETAIL 13/C6.
- 8) INSTALL SEDIMENT POND. LINE BOTTOM WITH 2-FT OF ORGANIC/TOPSOIL AND COMPACT TO 95% STANDARD PROCTOR.
- 9) FUTURE OUTDOOR RETAIL DISPLAY AREA. CURRENTLY PLANNED AS OPEN SPACE (TURF).
- 10) CONVENTIONAL SEPTIC DRAIN FIELD.
- 11) PROPOSED WELL LOCATION.
- 12) PROPOSED LOCKING GATE
- 13) PROPOSED VEGETATED 8-FT HIGH BERM (SEE LANDSCAPE PLAN FOR DETAILS)
- 14) EXISTING WETLAND TO REMAIN UNDISTURBED WITH MINIMUM 45-FT BUFFER FROM PROPOSED HARD SURFACES.
- 15) CONCRETE CURBED ISLAND FOR ELECTRICAL HOOKUP FOR SEMI TRAILERS. SEE B612 CURB DETAIL 7/C6
- 16) ABOVE GROUND STORAGE TANKS WITH REDUNDANT SPILL CONTAINMENT PER MPCA REQUIREMENTS.
- 17) MONUMENT SIGN (SEE ARCHITECTURAL FOR DETAIL OF SIGN. SETBACK TO PROPERTY LINE IS PROPOSED AT 10-FT.
- 18) CST TRUCK ENTRY, SEE SIGN DETAIL 11/C6.
- 19) CST TRUCK ENTRANCE AHEAD, SEE SIGN DETAIL 12/C6.
- 20) 6-FT HIGH CHAIN LINK FENCE WITH GREEN VINYL SLATS
- 21) 30-FT X 30-FT CONTAINER FOR EMPTY PALLETS.

LAYOUT NOTES:

- 1) ALL RADII TO BACK OF CURB
- 2) ALL DIMENSIONS TO THE FACE OF CURB UNLESS NOTED OTHERWISE.
- 3) PAVEMENT STRIPING TO BE 4" WIDE WHITE EPOXY PAINTED STRIPE.

SURFACE AREA ANALYSIS

TOTAL PROPERTY AREA (EXCLUDES COUNTY EASEMENT AREA)	= 1,660,500 Sq. Ft.	
PROPOSED IMPERVIOUS SURFACE	= 971,255 Sq. Ft. (58.5%)	
BIT. SURFACE	= 336,690 Sq. Ft.	
BUILDINGS	= 43,465 Sq. Ft. (Includes Expansion)	
GRAVEL	= 591,100 Sq. Ft.	
PROPOSED PERVIOUS SURFACE	= 689,245 Sq. Ft. (41.5%)	

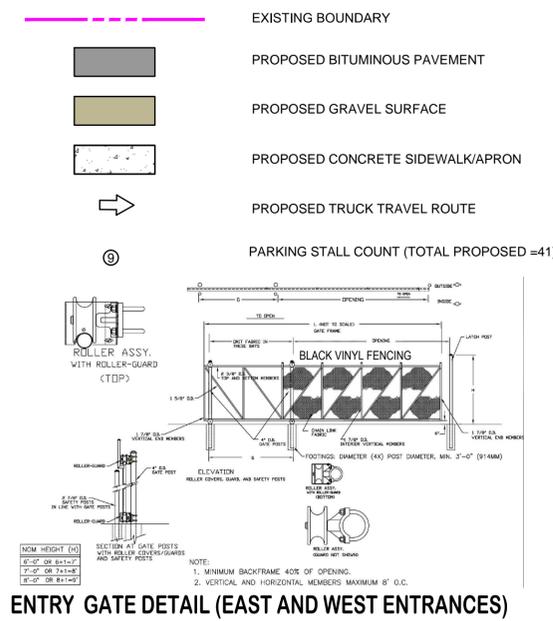
PARKING ANALYSIS:

OFFICE	3,746 SQ. FT. / 200 = 19	ITEM 25
WAREHOUSE	15,000 SQ. FT. / 1,700 = 9	ITEM 28
BAGGING	10,400 SQ. FT. / 1,700 = 6	ITEM 28
SERVICE / WASH BAY	3 STALLS X 2 + 1 = 7	ITEM 18
TOTAL REQUIRED	41	
TOTAL PROVIDED	41	

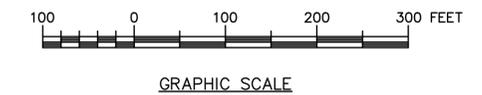
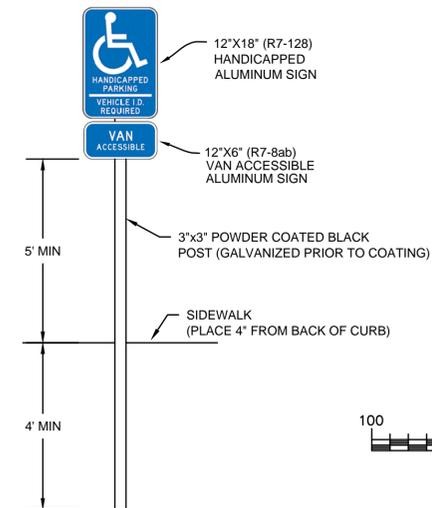
SETBACK REQUIREMENTS:

- 1) Front yard:
 - a) Local/collector street 40 feet
 - b) Arterial street 50 feet
 - c) State/county street 100 feet
- 2) Side yard 10 feet
- 3) Rear yard 25 feet, except 60 feet if abutting a residential district

LEGEND:

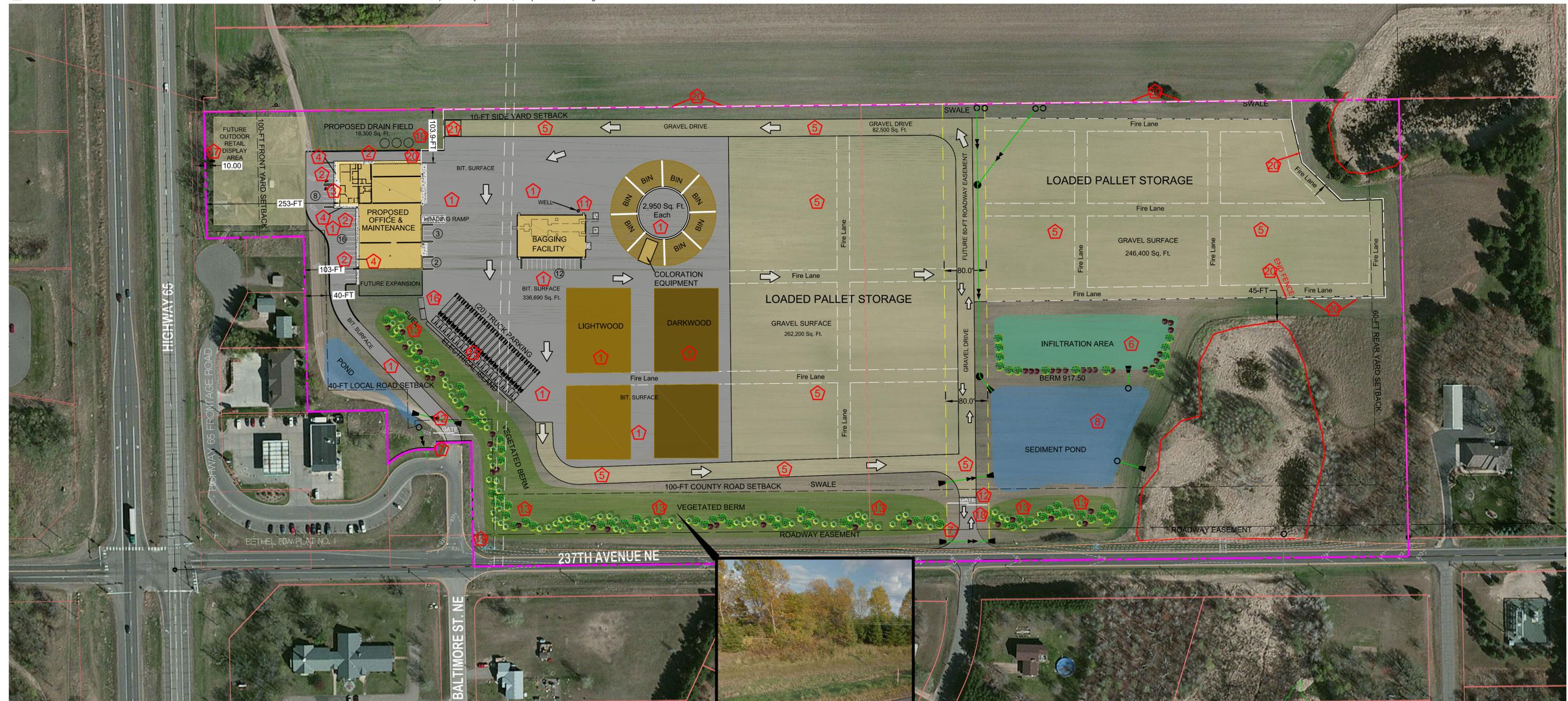


1 ADA SIGN INSTALLATION
C2 NO SCALE



OUTDOOR STORAGE AREA

- 1) DARK AND LIGHT WOOD MULCH = 74,000 SQ. FT.
- 2) PALLET STORAGE = 391,200 SQ. FT.
- 3) TOTAL OUTDOOR STORAGE = 465,200 SQ.FT. (10.7 ACRES)
- 4) TOTAL REAR YARD STORAGE = 28.1%



DEPICTION OF BERM

DRAWING PHASE:

OWNER REVIEW	
AGENCY REVIEW	✓
BID DOCUMENT	
PERMIT SET	
AS-BUILT DOCUMENT	

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TODD A. ERICKSON, PE
40418

LICENSE NO.
04/21/2016

DATE:

OWNER/DEVELOPER
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21897 S. Diamond Lake Rd. #400
Rogers, Minnesota 55374

PROJECT TITLE
CST COMPANIES, LLC
EAST BETHEL, MINNESOTA

NO.	REVISION DESCRIPTION	DATE

JOB NO. 16-107

SHEET TITLE
OVERALL LAYOUT PLAN

SHEET NO.

C2

KEYED NOTES:

- 1) INSTALL BITUMINOUS PAVEMENT. REFER TO DETAIL 1/C6 FOR SECTION.
- 2) INSTALL INTEGRAL CURB/SIDEWALK. REFER TO DETAIL 2/C6.
- 3) CONSTRUCT CONCRETE ADA RAMP. REFER TO DETAIL 5/C6 FOR LAYOUT AND 1/C2 FOR SIGN DETAIL.
- 4) INSTALL 4" CONCRETE SIDEWALK. REFER TO DETAIL 3/C6.
- 5) INSTALL B612 CONCRETE CURB AND GUTTER
- 6) INSTALL TRUCK DOCK CONCRETE SECTION (8" CONCRETE, 6,000 PSI, AIR ENTRAINED, FIBER MESH, OVER 6" CLASS 5 AGGREGATE BASE)
- 7) INSTALL TRUCK DOCK RAMP
- 8) PAINTED ISLAND

LAYOUT NOTES:

- 1) ALL RADII TO BACK OF CURB
- 2) ALL DIMENSIONS TO THE FACE OF CURB UNLESS NOTED OTHERWISE.
- 3) PAVEMENT STRIPING TO BE 4" WIDE WHITE EPOXY PAINTED STRIPE.

LEGEND:

-  EXISTING BOUNDARY
-  PROPOSED BITUMINOUS PAVEMENT
-  PROPOSED GRAVEL SURFACE
-  PROPOSED CONCRETE SIDEWALK/APRON

DRAWING PHASE:

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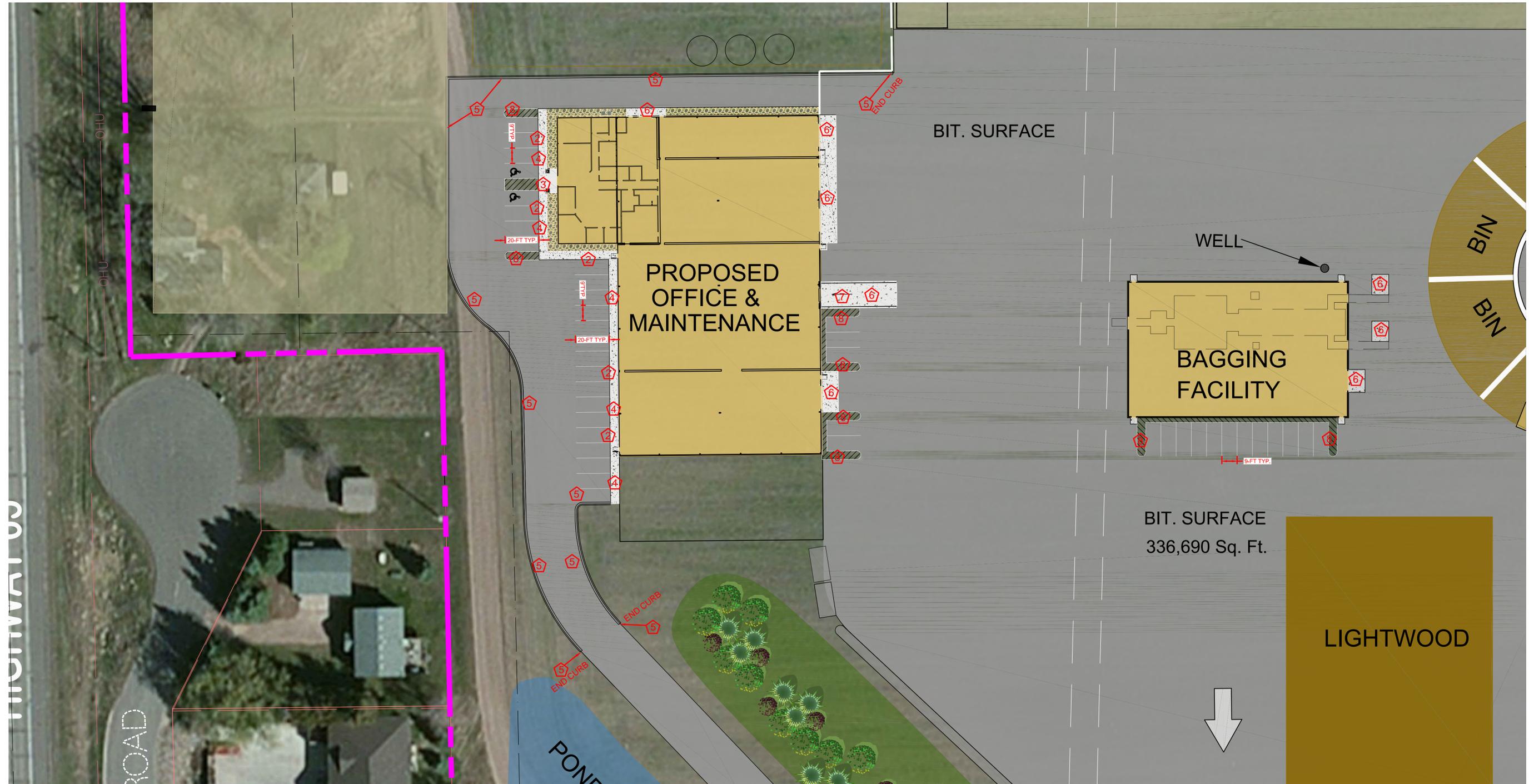
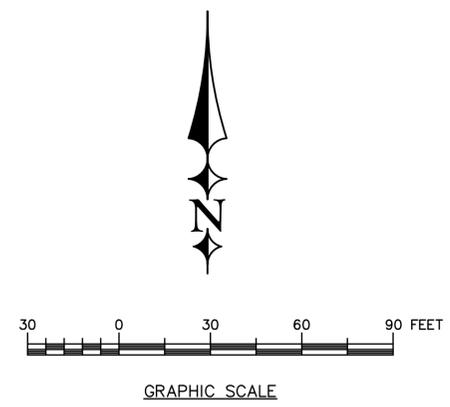
JOB NO. 16-107

SHEET TITLE
LAYOUT PLAN
SHOP/OFFICE
DETAIL

SHEET NO.
C3

SHEET 3 OF 6

38 of 110



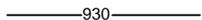
KEYED NOTES:

- 1) MATCH EXISTING BITUMINOUS PAVEMENT ELEVATIONS.
- 2) PROPOSED ROCK CONSTRUCTION ENTRANCE. REFER TO DETAIL 4/C5.
- 3) INSTALL CLASS III RIP RAP OVER GEOTEXTILE FABRIC (MNDOT 3733, TYPE IV).
- 4) INSTALL CLASS II RIP RAP OVER GEOTEXTILE FABRIC (MNDOT 3733, TYPE IV).
- 5) VEGETATIVE WEIR (COCONUT FIBER BLANKET) OVER BERM. BERM BETWEEN POND SHALL BE COMPACTED TO 100% STANDARD PROCTOR TO A DEPTH BELOW THE INFILTRATION BASIN. SEE DETAIL 7/C6.
- 6) INSTALL DOUBLE ROW SILT FENCING AROUND WETLAND
- 7) INSTALL SINGLE ROW SILT FENCING
- 8) INSTALL 9" BIO LOG IN SWALE/ON BANK
- 9) 3-FT WIDE CURB CUT (RIP RAP TO BOTTOM OF SWALE)

NOTES:

- 1) ALL CONSTRUCTION AS CALLED FOR ON THESE CONTRACT DOCUMENTS SHALL BE PERFORMED IN ACCORDANCE WITH ALL OSHA REQUIREMENTS.
- 2) THE GRADING CONTRACTOR SHALL SCHEDULE THE SOILS ENGINEER SO THAT CERTIFICATION OF ALL CONTROLLED FILLS WILL BE FURNISHED TO THE OWNER DURING AND UPON COMPLETION OF THE PROJECT.
- 3) SPOT ELEVATIONS/CONTOURS SHOWN AS FINISHED GRADE ELEVATIONS.
- 4) PRIOR TO ON SITE EXCAVATION OR DEMOLITION WORK, INSTALL EROSION CONTROL MEASURES IN LOCATIONS SHOWN OR AS DIRECTED BY THE ENGINEER OR CITY STAFF.
- 5) EROSION CONTROL MEASURES SHOWN ON THE EROSION CONTROL PLAN ARE THE ABSOLUTE MINIMUM. THE CONTRACTOR SHALL INSTALL SEDIMENT TRAPS OR BASINS AND BIO LOG AS DEEMED NECESSARY TO CONTROL EROSION.
- 6) GRADING OPERATIONS SHALL BE CONDUCTED IN A MANNER TO MINIMIZE THE POTENTIAL FOR SITE EROSION. SEDIMENT CONTROL PRACTICES MUST BE ESTABLISHED PRIOR TO THE START OF ANY UP GRADIENT LAND DISTURBING ACTIVITIES
- 7) PROVIDE 6" OF NATIVE TOPSOIL IN GREEN AREAS.
- 8) ALL EXPOSED SOIL AREAS MUST BE STABILIZED AS SOON AS POSSIBLE TO LIMIT SOIL EROSION BUT IN NO CASE LATER THAN 14 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
- 9) IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFF-SITE ACCUMULATIONS OF SEDIMENT MUST BE REMOVED IN A MANNER AND AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS.
- 10) TEMPORARY SOIL STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS, AND CANNOT BE PLACED IN SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS SWALES AND DITCHES UNLESS THERE IS A BYPASS IN PLACE FOR THE STORMWATER.
- 11) SLOPES 3:1 AND GREATER SHALL BE STABILIZED WITH EROSION CONTROL BLANKET.
- 12) MAINTAIN AND REPAIR EROSION CONTROL MEASURES (INCLUDING REMOVAL OF ACCUMULATED SILT) UNTIL VEGETATION IS ESTABLISHED. CONTRACTOR TO INSPECT AND DOCUMENT EROSION CONTROL DAILY AND AFTER ANY RAIN EVENT. ALL SEDIMENT CONTROL FEATURES MUST BE REPAIRED WHEN THE SEDIMENT REACHES 1/3 THE HEIGHT OF THE STRUCTURE OR REPLACED WITHIN 24 HOURS OF DISCOVERY. EROSION CONTROL STRUCTURES FOUND DAMAGED MUST BE REPAIRED OR REPLACED WITHIN 24 HOURS UPON DISCOVERY. REMOVAL OF EROSION CONTROL STRUCTURES REQUIRED AFTER SITE IS STABILIZED (AT DIRECTION OF ENGINEER).
- 13) ALL EXISTING CITY STREETS SHALL BE SWEEPED AS NEEDED AND AS REQUESTED BY ENGINEER OR CITY STAFF.
- 14) REFER TO GEOTECHNICAL REPORT FOR ADDITIONAL REQUIREMENTS.
- 15) AFTER GRADING OPERATIONS ARE COMPLETED, LANDSCAPE CONTRACTOR SHALL UNCOMPACT ALL GREEN AREAS PRIOR TO SODDING AND LANDSCAPING.
- 16) PERMANENT RESTORATION IN LAWN AREAS SHALL CONSIST OF PLACING SOD PER MNDOT 3878.2.A SPECIFICATIONS. SOD STRIPS SHALL NOT HAVE DEAD OR DRY EDGES AND SHALL NOT BE CUT MORE THAN 24 HOURS IN ADVANCE OF DELIVERY.
- 17) TEMPORARY STABILIZATION OF SLOPES AND GRADING AREAS DURING CONSTRUCTION SHALL BE A MNDOT 150 MIXTURE. TEMPORARY SEED MIXTURE SHALL BE PLACED WITH A DRILL AT A RATE OF 60 LBS/ACRE.
- 18) SOD AND INSTALLATION OF EROSION CONTROL BLANKET SHALL BE COMPLETED WITHIN 48 HOURS OF FINAL GRADING.
- 19) POSITIVE DRAINAGE OF MINIMUM 2% SLOPE SHALL BE ACHIEVED AWAY FROM PROPOSED BUILDING.

LEGEND:

-  EXISTING PROPERTY BOUNDARY
-  EXISTING 2-FT CONTOUR
-  PROPOSED 2-FT CONTOUR
-  PROPOSED SPOT ELEVATION
G = PROPOSED GROUND SURFACE
gl = GUTTER LINE
c = CONCRETE SURFACE
b = BITUMINOUS SURFACE
tw = TOP OF WALL
bw = BOTTOM OF WALL
NOTE: * REPRESENTS EXISTING GRADE.
-  PROPOSED HEAVY DUTY SILT FENCE
-  9" MNDOT SPEC. BIO LOG
-  WOOD FIBER BLANKET TYPE 2S

NOTE: ALL SLOPES 3.5:1 OR LESS

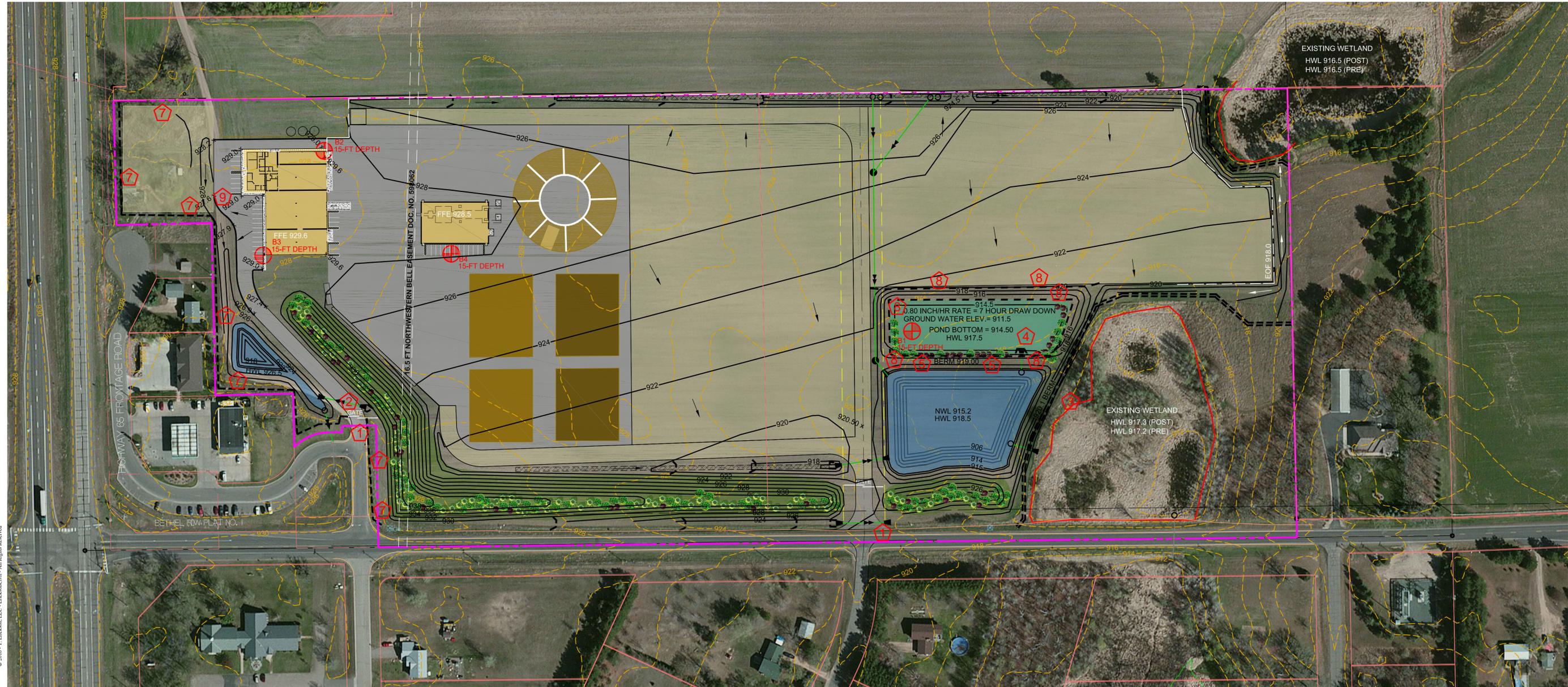


GRAPHIC SCALE



INFILTRATION BASIN CONSTRUCTION:

- 1) The contractor shall excavate the final fill material below elevation 914 and construct the infiltration basin after all the site improvements are in place including established vegetation. Estimated start date for this activity is fall of 2016.
- 2) Erosion control measures shall be left in place until site is 100% stabilized as determined by the Engineer.



DRAWING PHASE:

OWNER REVIEW
AGENCY REVIEW
BID DOCUMENT
PERMIT SET
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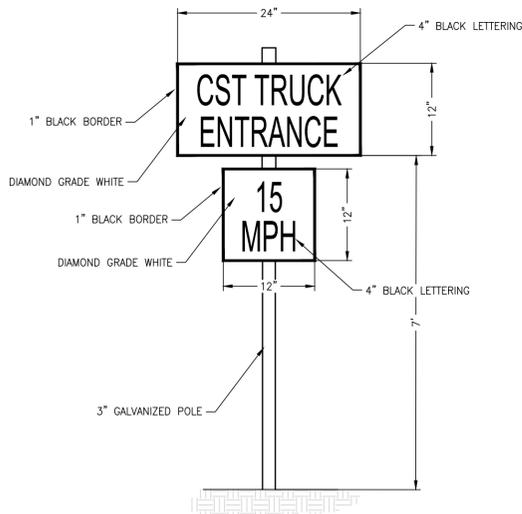
NO.	REVISION DESCRIPTION	DATE

JOB NO. 16-107

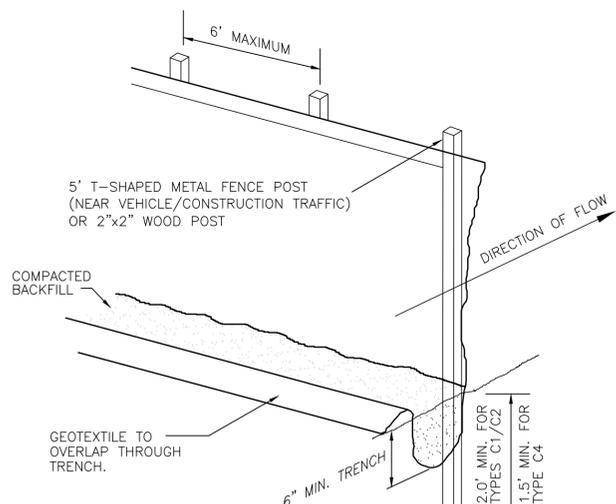
SHEET TITLE
GRADING PLAN

SHEET NO.

C4

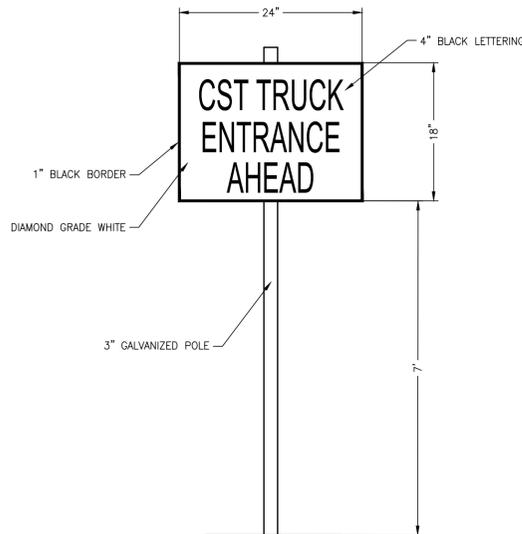


11 EAST TRUCK ENTRANCE SIGNAGE
C6 NO SCALE

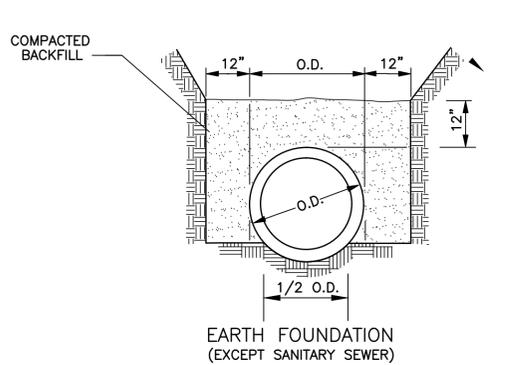


NOTE : -SILT FENCE INSTALLATION SHALL CONFORM TO MNDOT2573.3, TYPE C1/C2 NEAR VEHICLE/CONSTRUCTION TRAFFIC, TYPE C4 AT ALL OTHER LOCATIONS.
-MATERIALS SHALL CONFORM TO MNDOT 3886.

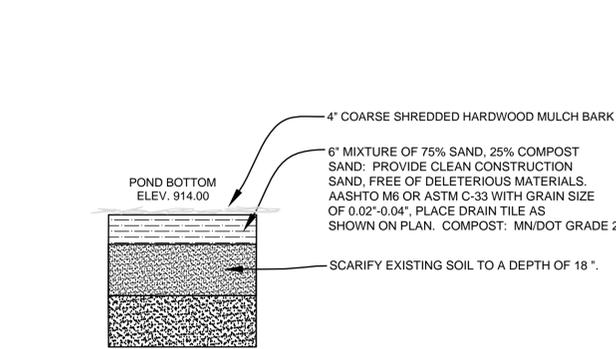
8 SILT FENCE
C6 NO SCALE



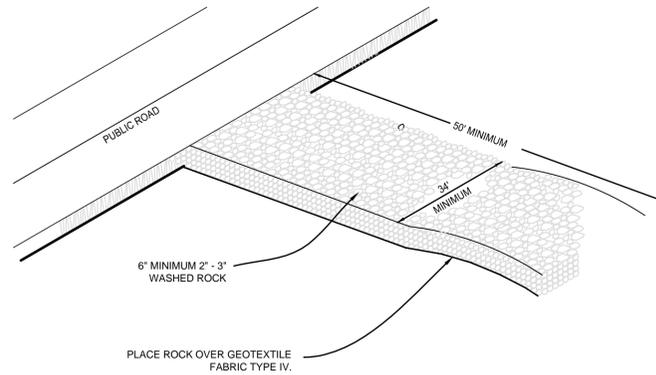
12 WEST DIRECTION AL SIGNAGE
C6 NO SCALE



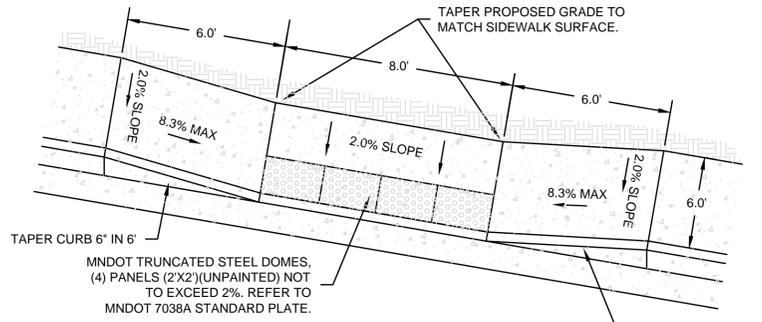
9 PIPE FOUNDATION DETAILS
C6 NO SCALE



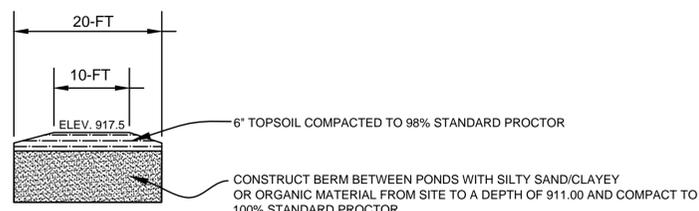
10 INFILTRATION SECTION AND INFILTRATION BASIN
C6 NO SCALE



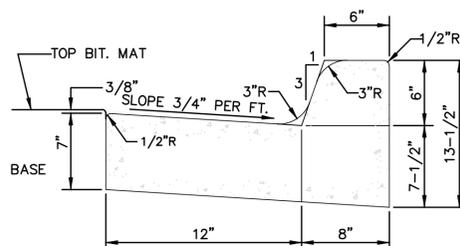
4 ROCK CONSTRUCTION ENTRANCE
C6 NO SCALE



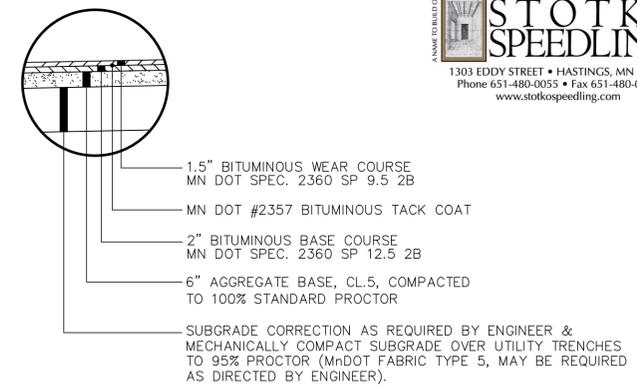
5 PARALLEL CURB RAMP
C5 NO SCALE



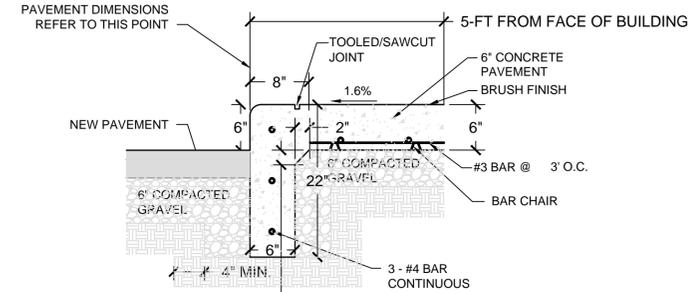
6 BERM BETWEEN POND AND INFILTRATION BASIN
C6 NO SCALE



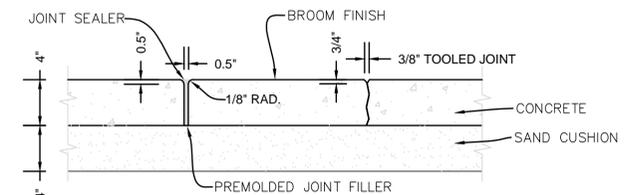
7 B612 CURB & GUTTER
C6 NO SCALE



1 BITUMINOUS PAVING SECTION
C6 NO SCALE



2 INTEGRAL CURB
C5 NO SCALE



NOTES:
TOOLED JOINTS:
1) JOINT SPACING SHALL MATCH WALK WIDTH UNLESS SHOWN OTHERWISE ON PLANS.
EXPANSION JOINTS:
1) WHERE WALK BUTTS ANY FIXED OBJECT SUCH AS WALLS, CURBS, MANHOLES, ETC.
2) 45° ON CENTER MAXIMUM OR AS SHOWN ON PLANS.
3) JOINT SEALER SHALL MEET ASTM D-412, GRAY, SELF LEVELING, EPOXY, AS WITH "QUICKJOINT 300" OR EQUAL.

3 CONCRETE SIDEWALK DETAIL
C6 NO SCALE

DRAWING PHASE:	
OWNER REVIEW	
AGENCY REVIEW	✓
BID DOCUMENT	
PERMIT SET	
AS-BUILT DOCUMENT	

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21897 S. Diamond Lake Rd. #400
Rogers, Minnesota 55374

CST COMPANIES, LLC
EAST BETHEL, MINNESOTA

PROJECT TITLE

DATE

REVISION DESCRIPTION

NO.

JOB NO. 16-107

SHEET TITLE
DETAILS

SHEET NO.

C6

SHEET 6 OF 6
41 of 110

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OWNER REVIEW	
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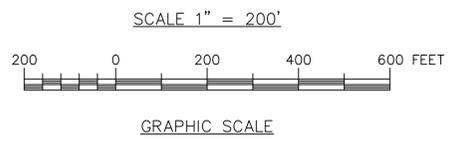
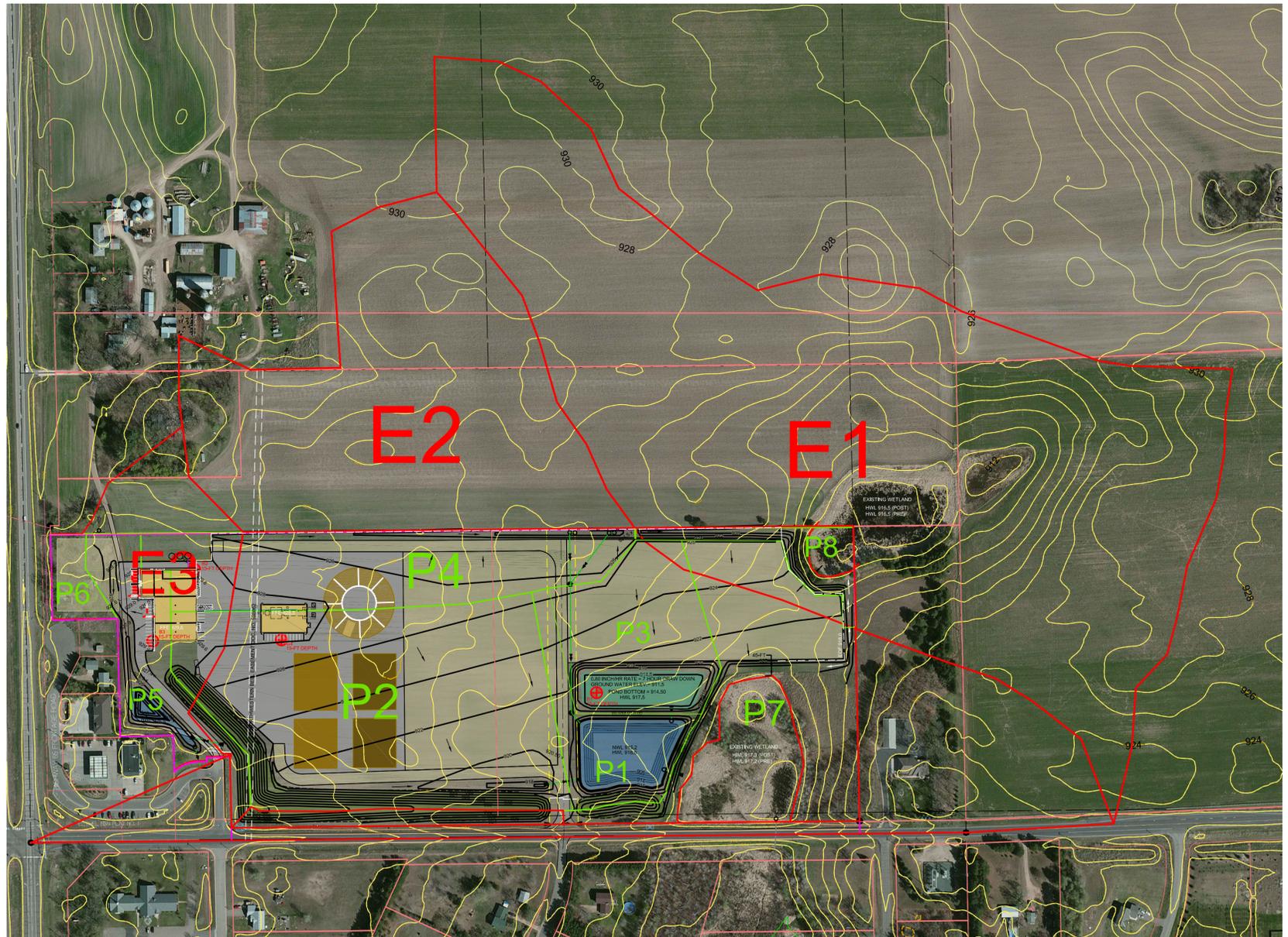
NO.	REVISION DESCRIPTION	DATE

JOB NO. **16-107**

SHEET TITLE
DRAINAGE MAP AND SUMMARY

SHEET NO.
D1

DRAINAGE MAP



LEGEND:

- 912 EXISTING 1-FT CONTOUR INTERVAL
- 914 PROPOSED 2-FT CONTOUR INTERVAL
- DRAINAGE ARROW
- PROPOSED DRAINAGE AREA
- EXISTING DRAINAGE AREA

NOTES:

- 1) AN INFILTRATION RATE OF 0.8 IN/HR WAS USED FOR THE INFILTRATION BASIN. THIS WILL BE CONFIRMED WITH A SOIL BORING TAKEN AT SITE OF INFILTRATION BASIN.

SURFACE AREA ANALYSIS

TOTAL PROPERTY AREA	= 1,660,500 Sq. Ft.
PROPOSED IMPERVIOUS SURFACE	= 971,255 Sq. Ft. (58.5%)
BIT. SURFACE	= 336,690 Sq. Ft.
BUILDINGS	= 43,465 Sq. Ft. (Includes Expansion)
GRAVEL	= 591,100 Sq. Ft.
PROPOSED PERVIOUS SURFACE	= 689,245 Sq. Ft. (41.5%)

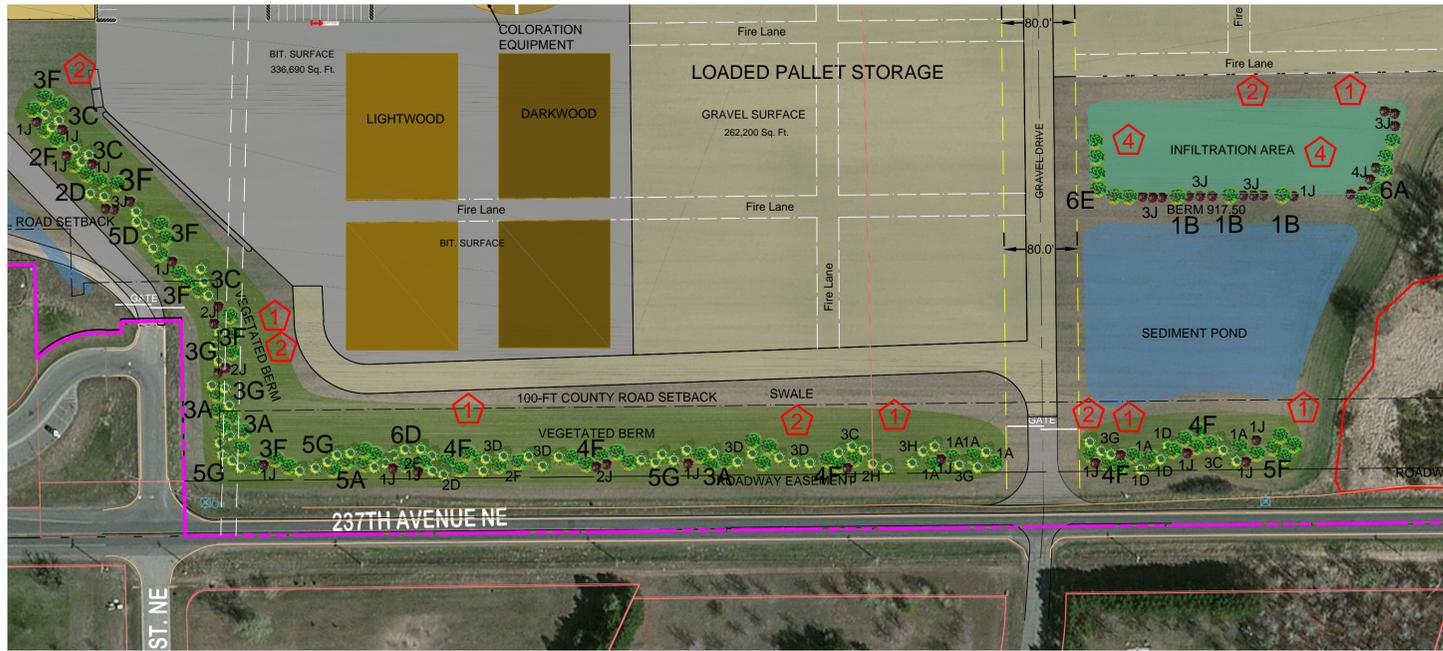
RUNOFF SUMMARY COMPARISON

STORM EVENT	RATE DISCHARGE COMPARISON (CFS)	
	EXISTING TOTAL	PROPOSED TOTAL
2-YEAR	4.83	3.29
10-YEAR	12.76	6.50
100-YEAR	33.57	16.69

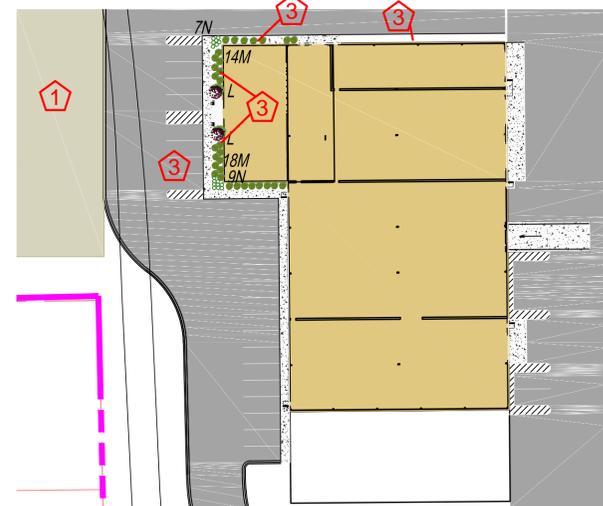
MPCA - 1.1 Inch Event

NEW IMPERVIOUSNESS = 971,255 Sq. Ft. X 1.1 Inches = 89,035 Cu. Ft.
 VOLUME TO INFILTRATE = 89,035 Cu. Ft., INFILTRATION VOLUME = 2.533 ACRE FT - 110,337 CU. FT. > 89,035 CU. FT.
 INFILTRATION RATE = 0.80 INCH/HOUR

BERM AND INFILTRATION BASIN PLANTING PLAN



OFFICE PLANTING DETAIL



PLANT MATERIALS SCHEDULE

TREES						
KEY	QTY.	BOTANICAL / COMMON NAME	SIZE	ROOT	SPACING	NOTES
A	29	Red Maple <i>Acer rubrum</i>	#25	Container	As Shown	
E	6	River Birch <i>Betula nigra</i>	8' ht.	B&B	As Shown	Clump form
D	31	Eastern Red Cedar <i>Juniperus virginiana</i>	6' ht.	B&B	As Shown	
G	27	White Spruce <i>Picea glauca</i>	6' ht.	B&B	As Shown	
H	5	Red Pine <i>Pinus resinosa</i>	6' ht.	B&B	As Shown	
F	47	Quaking Aspen <i>Populus tremuloides</i>	#10	Container	As Shown	
B	3	Northern Pin Oak <i>Quercus ellipsoidalis</i>	#25	Container	As Shown	
C	15	Techny Arborvitae <i>Thuja occidentalis 'Techny'</i>	6' ht.	B&B	As Shown	

SHRUBS						
KEY	QTY.	BOTANICAL / COMMON NAME	SIZE	ROOT	SPACING	NOTES
J	42	Cardinal Dogwood <i>Cornus sericea 'Cardinal'</i>	#3	Container	As Shown	
L	2	Diablo Ninebark <i>Physocarpus opulifolius 'Donna May'</i>	#3	Container	As Shown	
M	24	Tor Birchleaf Spirea <i>Spiraea betulifolia 'Tor'</i>	#3	Container	As Shown	

PERENNIALS						
KEY	QTY.	BOTANICAL / COMMON NAME	SIZE	ROOT	SPACING	NOTES
N	21	Karl Foerster Feather Reed Grass <i>Calamagrostis x acutiflora 'Karl Foerster'</i>	#1	Container	As Shown	

Notes:
1- Trees shall be of quality prescribed in crown observations and root observations details and specifications.
2- See specifications for further requirements related to this detail.

Trunk caliper shall meet ANSI Z60 current edition for root ball size.
Root ball modified as required.
Round-topped soil berm 4" high x 8" wide above root ball surface shall be centered on the downhill side of the root ball for 240". Berm shall begin at root ball periphery.

Modified soil. Depth varies. (See soil preparation plan).

DECIDUOUS TREE PLANTING

Notes:
1- Trees shall be of quality prescribed in crown observations and root observations details and specifications.
2- See specifications for further requirements related to this detail.

Trunk caliper shall meet ANSI Z60 current edition for root ball size.
Root ball modified as required.
Round-topped soil berm 4" high x 8" wide above root ball surface shall be centered on the downhill side of the root ball for 240". Berm shall begin at root ball periphery.

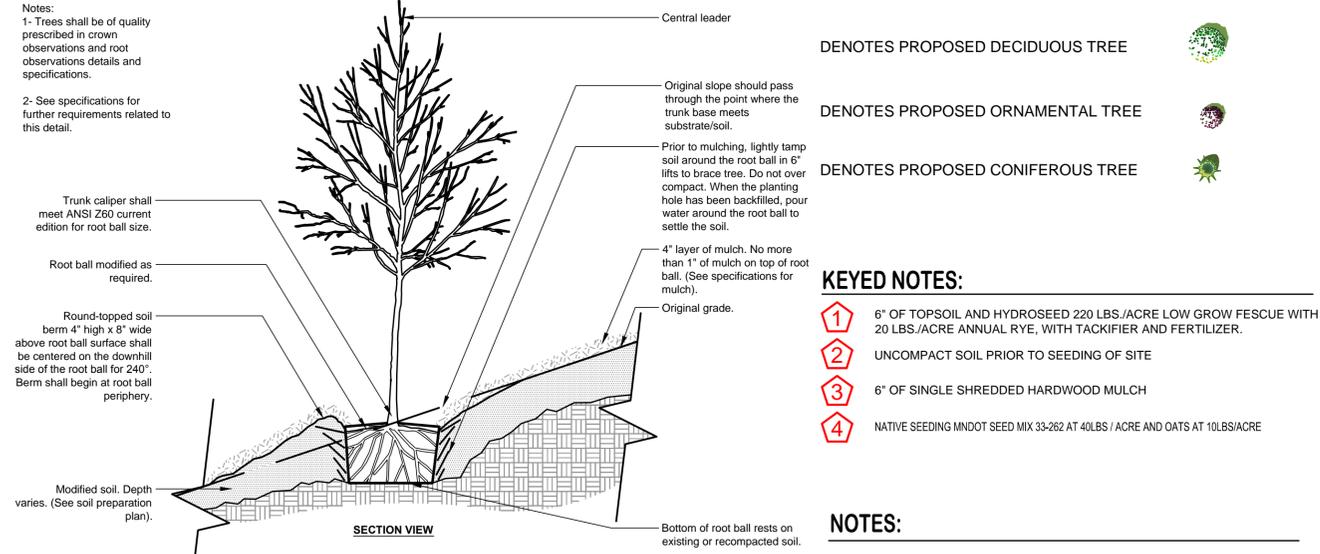
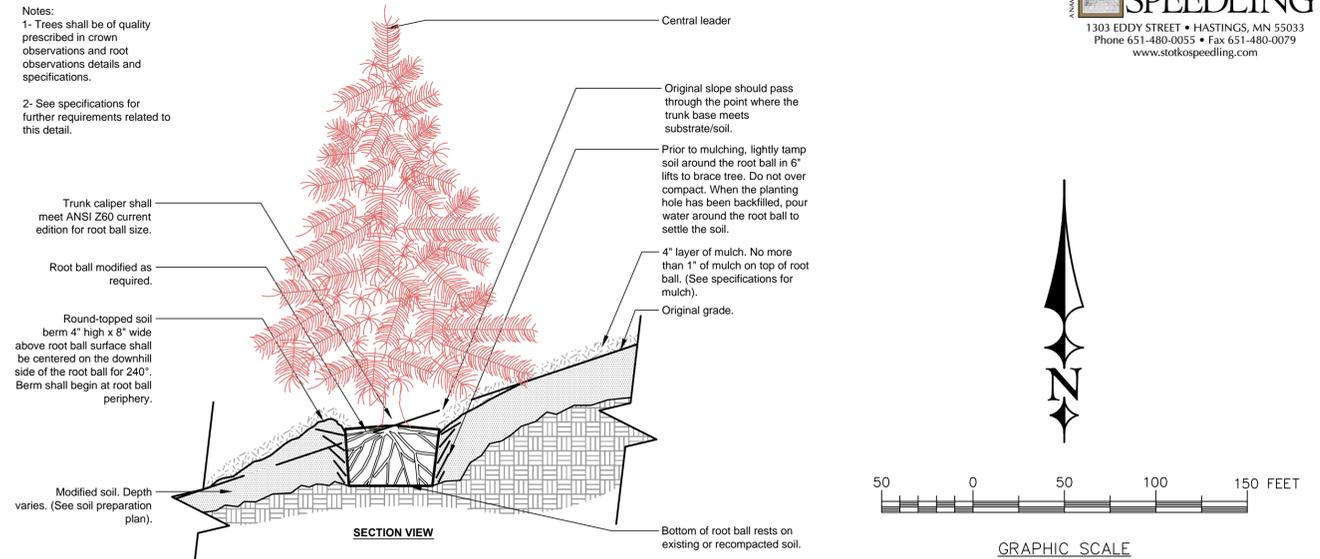
Modified soil. Depth varies. (See soil preparation plan).

CONIFEROUS TREE PLANTING

4" layer of mulch. No more than 1" of mulch on top of root ball. (See specifications for mulch).
Original slope should pass through the point where the trunk meets substrate/soil.
Modified soil. Depth varies. (See specifications for soil modification).
Bottom of root ball rests on existing or recompacted soil.

Notes:
1- Shrubs shall be of quality as prescribed in the root observations detail and specification.
2- See specifications for further requirements related to this detail.

SHRUB PLANTING



LEGEND:

- EXTERIOR PROPERTY BOUNDARY
- DENOTES PROPOSED DECIDUOUS TREE
- DENOTES PROPOSED ORNAMENTAL TREE
- DENOTES PROPOSED CONIFEROUS TREE

KEYED NOTES:

- 1 6" OF TOPSOIL AND HYDROSEED 220 LBS./ACRE LOW GROW FESCUE WITH 20 LBS./ACRE ANNUAL RYE, WITH TACKIFIER AND FERTILIZER.
- 2 UNCOMPACT SOIL PRIOR TO SEEDING OF SITE
- 3 6" OF SINGLE SHREDDED HARDWOOD MULCH
- 4 NATIVE SEEDING MNDOT SEED MIX 33-262 AT 40LBS / ACRE AND OATS AT 10LBS/ACRE

NOTES:

- ALL PLANTING BEDS SHALL RECEIVE MIN. 6" TOPSOIL, HEAVY LANDSCAPE FABRIC AND BROWN METAL EDGING WHERE MULCH MEETS LAWN.
- ALL LAWN AREAS SHALL RECEIVE A MINIMUM OF 6" OF TOPSOIL
- ENTIRE SITE SHALL BE IRRIGATED. DESIGN/BUILD BY CONTRACTOR.
- SEE PLAN FOR AREAS OF SEED AND BLANKET AREAS. AREAS CALLED OUT FOR SEEDING SHALL RECEIVE LOW GROW FESCUE MIX FROM TWIN CITY SEED COMPANY AND BE SEED AT A RATE OF 220 LBS. PER ACRE, W/ 20LBS/ACRE ANNUAL RYE.
- BUILDING CONTRACTOR SHALL INCLUDE COST OF ELECTRICAL AND PLUMBING FOR INSTALLATION OF IRRIGATION SYSTEM. IRRIGATION CONTROL BOX SHALL BE BY LANDSCAPE CONTRACTOR.
- PERENNIAL AND ANNUAL PLANTING AREAS SHALL RECEIVE 6" OF SINGLE SHREDDED HARDWOOD MULCH WITH NO FABRIC. SAMPLE SHALL BE SUBMITTED FOR APPROVAL PRIOR TO PLACEMENT.

STOTKO SPEEDLING
1303 EDDY STREET • HASTINGS, MN 55033
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SHEET TITLE
LANDSCAPE PLAN
SHEET NO.
L1
SHEET 1 OF 1
44 of 110

CERTIFICATE OF SURVEY

LYING IN THE SE 1/4 OF SEC. 29, TOWNSHIP 34, RANGE 23 ANOKA COUNTY, MINNESOTA

PROPERTY ADDRESS:
23805 HIGHWAY 65
East Bethel, Minnesota
PROPERTY ID 29-34-23-43-0012

PREPARED FOR:
CST Companies, LLC
21897 S. Diamond Lake Road #400
Rogers, Minnesota 55374



LEGEND

- DENOTES 3/4 INCH X 18 INCH IRON PIPE MONUMENT TO BE SET IN ACCORDANCE WITH MS 505.021, SUBD. 10, MARKED BY LICENSE NO. 49506
- ⊙ DENOTES PK NAIL TO BE SET IN ACCORDANCE WITH MS 505.021, SUBD. 10, WITH STAINLESS STEEL MARKER STAMPED BY LICENSE NO. 49506
- DENOTES IRON MONUMENT FOUND
- ⊙ DENOTES ANOKA COUNTY CAST IRON MONUMENT

LEGAL DESCRIPTION

THAT PART OF LOTS 7 & 8, AUDITOR'S SUBDIVISION NO. 130, LYING SOUTH OF A LINE 457.42 FEET SOUTH OF AND PARALLEL TO NORTH LINE OF SAID LOTS; EXCEPT THAT PART OF SAID LOT 7 THAT LIES WITHIN THE FOLLOWING DESCRIBED TRACT: THAT PART OF SW 1/4 OF SE 1/4 OF SECTION 29 TOWNSHIP 34 RANGE 23 DESCRIBED AS FOLLOWS: COMMENCING AT SOUTHWEST CORNER OF SAID SW 1/4 OF SE 1/4, THENCE EASTERLY ALONG SOUTH LINE OF SAID SW 1/4 OF SE 1/4 TO EAST LINE OF WEST 50 FT OF SAID SW 1/4 OF SE 1/4, THENCE NORTHERLY ALONG SAID EAST LINE OF WEST 50 FT 148.71 FEET TO POINT OF BEGINNING, THENCE CONTINUE NORTHERLY ALONG SAID EAST LINE 145 FEET, THENCE EASTERLY AT RIGHT ANGLE TO SAID EAST LINE 358.71 FEET, THENCE SOUTHERLY PARALLEL WITH WEST LINE OF SAID SW 1/4 OF SE 1/4 298.04 FEET, PLUS OR MINUS, TO THE SOUTH LINE OF SAID SW 1/4 OF SE 1/4, THENCE WESTERLY ALONG SAID SOUTH LINE 150.03 FT, PLUS OR MINUS, TO A POINT THAT IS 258.71 FEET EASTERLY OF SOUTHWEST CORNER OF SAID SW 1/4 OF SE 1/4, AS MEASURED ALONG SAID SOUTH LINE, THENCE NORTHERLY PARALLEL WITH SAID WESTERLY LINE 148.71 FEET, THENCE WESTERLY PARALLEL WITH SAID SOUTH LINE 208.71 FEET TO POINT OF BEGINNING, EXCEPT THAT PART OF EAST 300 FEET OF SAID LOT 8 LYING SOUTHERLY OF NORTH 457.42 FEET THEREOF, EXCEPT ROAD(S) AND SUBJECT TO EASEMENTS OF RECORD, ANOKA COUNTY, MINNESOTA.

SUBJECT TO EASEMENTS OF RECORD.
SUBJECT TO THE RIGHTS OF THE PUBLIC IN C.S.A.H. NO. 24
CONTAINING 40.56 ACRES, MORE OR LESS.

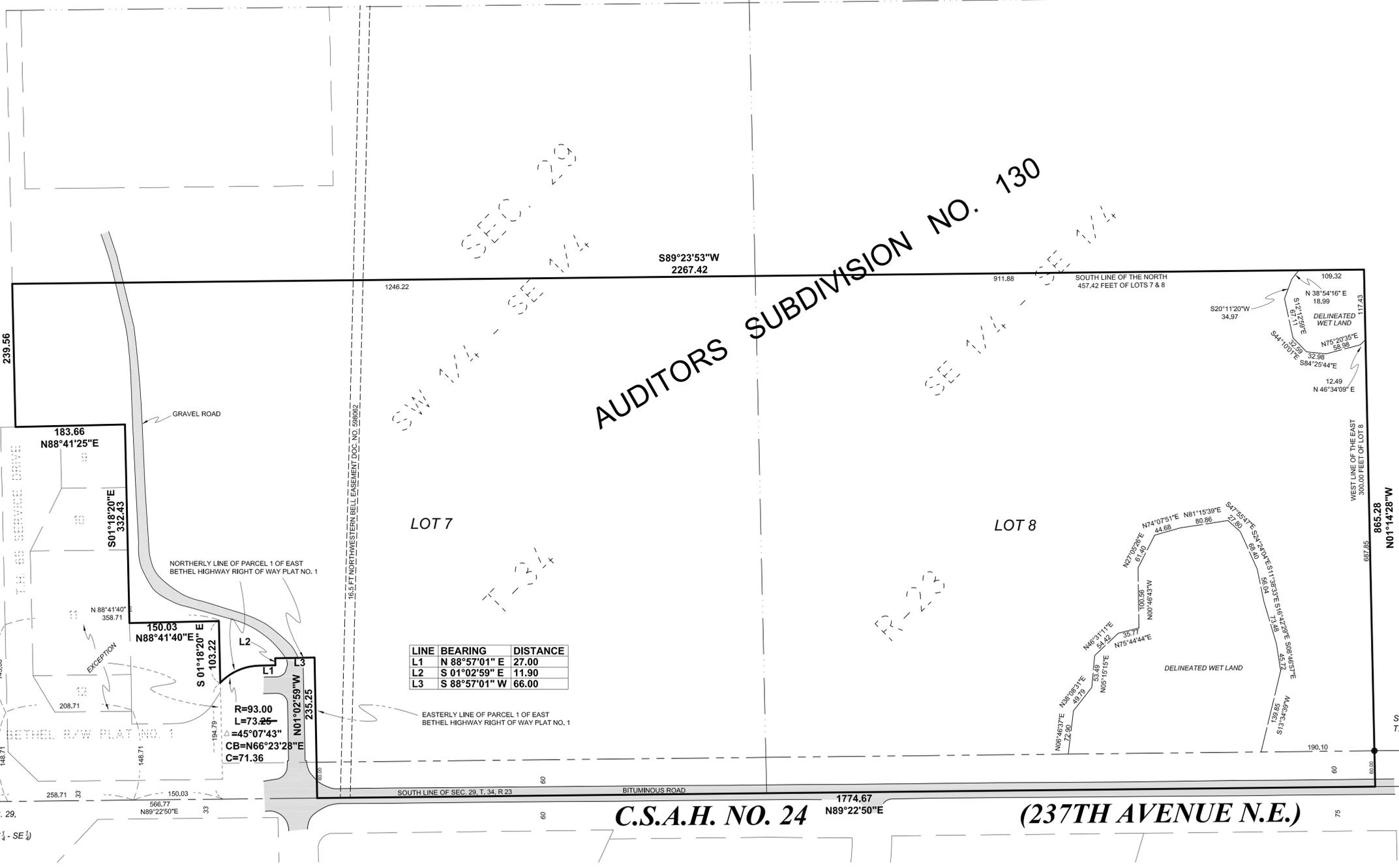
*Surveyors Notes: Legal description above does not except property acquired by the CITY OF EAST BETHEL HIGHWAY RIGHT-OF-WAY PLAT NO. 1

W 1/4 COR. SEC. 29,
T. 34, R. 23

WEST LINE OF SEC. 29, T. 34, R. 23
S01°18'20"E
239.56

S.T.H. NO. 65
N 00°18'20"W
145.00
N 01°18'20"W
148.71
N 89°22'50"E
50.00

SW COR. SEC. 29,
T. 34, R. 23
(SW COR. SW 1/4 - SE 1/4)



SEC. 29
SW 1/4 - SE 1/4

AUDITOR'S SUBDIVISION NO. 130

LOT 7

LOT 8

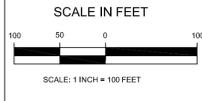
LINE	BEARING	DISTANCE
L1	N 88°57'01" E	27.00
L2	S 01°02'59" E	11.90
L3	S 88°57'01" W	66.00

EASTERLY LINE OF PARCEL 1 OF EAST
BETHEL HIGHWAY RIGHT OF WAY PLAT NO. 1

C.S.A.H. NO. 24 (237TH AVENUE N.E.)

Mar 31, 2016 - 3:40pm - C:\SURVEY\2016\2016-16\CERT.dwg

NOTE: THIS SURVEY WAS INTENDED ONLY FOR THE BENEFIT OF THE PARTY TO WHOM IT WAS PREPARED FOR AND SHOULD NOT BE RELIED UPON BY AN OTHER PARTY OR FOR ANY OTHER PURPOSE WITHOUT FIRST CONTACTING THE SURVEYOR WHO DEVELOPED AND MADE THIS DRAWING. UNAUTHORIZED REPRODUCTION OF THIS DOCUMENT IS PROHIBITED.



I HEREBY CERTIFY THAT THIS SURVEY, PLAN, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED LAND SURVEYOR UNDER THE LAWS OF THE STATE OF MINNESOTA.

JESSE T. BENGTON LIC. NO. 49506 DATE: 03/30/2016

DESIGNED BY:
DRAWN BY:
CHECKED BY:

TRUE NORTH SURVEYS, P.A.
21 1ST AVE. SW, P.O. BOX 264
NEW LONDON, MN 56273
Phone: (320)-212-1089

JOB NUMBER: 2016-16
LOCATION: SEC. 29, T-34, R-23
CLIENT: ERICKSON CIVIL

SHEET 1
OF
1 SHEETS

CST Companies, LLC

Site Plan Narrative

April 21, 2016

PROJECT DESCRIPTION

The CST Companies project is located on 237th Avenue NE, and also fronts Highway 65 in East Bethel. The property is approximately 38.1 acres in size and is currently farmed. The current zoning for the parcel is I-1, Light Industrial. The majority of the site is open farm land with two wetlands in the southeast and northeast portion of the property. There is also one single family home located in the northwest portion of the property. The proposed use is a permitted use under the code.

The proposed use will encompass 58.5% impervious surface of the property with the remainder utilized for storm water ponding, septic, preservation of the existing wetlands, berming and drainage swales.

The site access is shown with one existing access point on Baltimore Street and the second access located further east at the existing Davenport Street location. The Baltimore Street access will be limited to an employee and future retail access point.

The Davenport Street access will provide the main truck access for the site. An agreement is proposed to be entered into with the City concerning the future potential extension of Davenport Street. The developer has agreed to provide an easement of 80 feet in width, if and when Davenport Street would be extended to serve the parcel to the north. Platting of the property or conveyance of easements are not proposed at this time.

A 26,745 sq. ft. Office and Maintenance Facility is proposed in the northwest portion of the property. A 6,000 sq. ft. future addition is also identified on the site plan. Directly to the east, an additional 10,720 sq. ft. Mulch Bagging Facility is proposed that will house the bagging operation of the colored and raw mulch product. The remainder of the paved surface area will include storage for raw mulch, bins for the coloration of mulch and a defined area for truck parking.

The remainder of the utilized hard surface area will be used for storage of pallets. This area will be a gravel surface as defined on the site layout plan. The area defined as the travel route is proposed as gravel with this plan, with the intent to pave the travel surface after a season of truck activity. Fire access lanes have been identified that will be left open for use by fire vehicles.

UTILITIES

The site will be serviced with a single well for both the coloration and for the operations of the truck maintenance and office facility. Septic for the site will be provided by an on-site drain field located north of the proposed office. The area defined for this use is over 18,000 sq. ft. This area will require testing and a design for the system to meet with the requirements of the County.

STORM SEWER, PONDING AND WETLANDS

The site is currently designed to meet/exceed the requirements of rate and volume control for the proposed operation. The ponding system has been designed assuming all hard surface areas, including gravel, defined as a bituminous surface, so no additional ponding or calculations would be required if the owner chose to pave the entire surface. An offline infiltration area has also been defined to meet the MPCA 1.1 inch volume control rule.

Based on the number of employees and with the expected use of bathroom, wash facilities and accounting for irrigation use, the office operation is expected to utilize an additional 0.5 million gallons per year. The coloration facility is expected to utilize a 1.7 million gallons of water.

This amount of water used is greatly offset from the proposed site storm water drainage system, which incorporates infiltration into the design. Also, taking this parcel out of crop production and infiltrating the water as designed greatly offsets the overall estimated use. The total rainfall for this area of the state averages 29 inches of rainfall. The average yearly infiltration for the system is therefore estimated at 19.7 acre-ft or 6.4 million gallons. This amount less 2.2 million gallons estimated for use

in operations, nets 4.2 million gallons of additional water that previously ran off from the site or was used by the crops to infiltrate and replenish the local groundwater.

Not taken into account with this calculation are the long linear swales that will also allow for good treatment, extended time of concentration of the water and additional infiltration potential prior to entering the ponding area and wetlands.

The existing wetlands have been incorporated into the design and the proposed plan provides for typical bounce within 0.1-ft of the existing conditions. The proposed grading will maintain the natural connection between the wetlands as it exists today, however it is not expected to be utilized, based on the modeling. With the warm spring this year, our wetland biologist was able to do a site evaluation and locate the boundary of the wetlands on the site. A buffer has been provided surrounding the wetlands and a double row of silt fencing has been incorporated into the plan to prevent any sediment from reaching the wetlands. A joint application has been completed and distributed to the various wetland review authorities at the time of this submission and has been accepted.

The water used for coloration and any excess colored water has been designed to flow back through the coloration pile and be absorbed by the mulch to the north of the coloration equipment. Typical loss for a coloration run is approximately 5 gallons of water, which is easily absorbed by the mulch surrounding the equipment. This off colored mulch is then utilized or colored as black mulch. This equates to a very eco-friendly and efficient operation.

BUFFERING AND LANDSCAPING

The site is currently designed with a naturalized 8-ft vegetative berm. The site's remaining open area, including the berm are planned to be seeded with a low grow fescue grass that is adaptable to this soil type and requires no irrigation. The infiltration area is also planned to be vegetated with a defined MnDOT seed mix appropriate for this type of use. The infiltration area will also be planted with trees that grow well in wet conditions. Landscaping will also be provided for the office building.

SITE LIGHTING

The sites lighting is proposed to be wall mounted LED units located around each of the proposed buildings. Additional lighting may be required, but due to the size of this site it is stated that no light will be cast past the property limits.

CST COMPANIES, LLC

SITE PLAN NARRATIVE

Page 4 of 5

ENGINEER/PLANNER/SURVEYOR/LANDSCAPE

EricksonCivil

Todd A. Erickson, PE

Greg Lundquist, LS

Stephen Mastey, LA

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BUILDER/PROJECT MANAGER

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Hastings, MN 55033

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Cell 651-329-2021

www.stotkospeedling.com

PROJECT MEMO

Project: CST East Bethel

To: Colleen Winter, City of East Bethel

From: Todd A. Erickson, PE

Date: 04-21-2016

Re: Water Appropriation Permit

Ms. Winter:

Below is my understanding for the anticipated water used for the CST Project based on my review of the current water and expected water use of the mulch coloration process and day to day operations of staff.

The mulch coloration process requires on average 47.5 Gallons Per Minute/Cu. Yds. during production. Production of mulch is estimated at 35,000 cu. yds. (Not all mulch is colored, some is sold as a natural color).

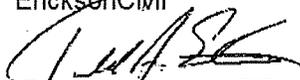
This equates 1,662,500 gallons of water used to manufacture the mulch. An additional 127 gallons/day per employee is expected from the overall operation. Using 15 full time staff (1/2 of part time seasonal staff + full time staff) for 5 days/week times 52 weeks a year, amounts to an additional 495,300 gallons per year. This per day amount includes bathroom and irrigation needs on average.

The total expected water usage for the ongoing operations is therefore 2,157,800 gallons, greater than 1 million gallons, thus requiring a Minnesota DNR Appropriation Permit.

As stated in our previous submitted narrative the infiltration basin as proposed is expected to more than offset this use with the expected infiltration of 6,419,000 gallons/year.

Please contact me if you have any questions.

Sincerely,
Erickson Civil



Todd A. Erickson, PE

CST Companies, LLC

Site Plan Narrative

March 25, 2016

PROJECT DESCRIPTION

The CST Companies project is located on 237th Avenue NE, and also fronts Highway 65 in East Bethel. The property is approximately 39.2 acres in size and is currently farmed. The current zoning for the parcel is I-1, Light Industrial. The majority of the site is open farm land with two wetlands in the southeast and northeast portion of the property. There is also one single family home located in the northwest portion of the property. The proposed use is a permitted use under the code.

The proposed use will encompass 56.85% of the property, with the remainder utilized for storm water ponding, preservation of the existing wetlands, berming and drainage swales.

The site access is shown with one existing access point on Baltimore Street and the second access located further east at the existing Davenport Street location. The Baltimore Street access will be limited to an employee and future retail access point. The Davenport Street access will provide the main truck access for the site.

A 26,745 Sq. Ft. Office and Maintenance Facility is proposed in the northwest portion of the property. A 6,000 Sq. Ft. future addition is also identified on the site plan. Directly to the east, an additional 10,720 Sq. Ft. Mulch Bagging Facility is proposed that will house the bagging operation of the colored and raw mulch product. The remainder of the paved surface area will include storage for raw mulch, bins for the coloration of mulch and a defined area for truck parking.

The remainder of the utilized hard surface area will be used for storage of pallets. This area will be a gravel surface as defined on the site layout plan. The area defined as the travel route is proposed as gravel with this plan, with the plan to pave the travel surface after a season of truck activity. Fire access lanes have been identified that will be left open for use by fire vehicles.

UTILITIES

The site will be serviced with a single well for both the coloration and for the operations of the truck maintenance and office facility. Septic for the site will be provided by an on-site drain field located north of the proposed office. The area defined for this use is over 18,000 sq. ft. This are will require testing and a design for the system to meet with the requirements of the County.

STORM SEWER, PONDING AND WETLANDS

The site is currently designed to meet/exceed the requirements of rate and volume control for the proposed operation. The ponding system has been designed assuming all hard surface areas, including gravel, defined as a bituminous surface, so no additional ponding or calculations would be required if the owner chose to pave the entire surface. An offline infiltration area has also been defined to meet the MPCA 1.1 inch volume control rule.

The proposed facility is expected to utilize 1.3 million gallons of water. This was a factor in the design of the drainage system. With the amount of infiltration provided, the water used by this facility will be greatly offset with average precipitation totals for this area of the State.

Taking this parcel out of crop production and infiltrating the water as designed greatly offsets the overall estimated use. The total rainfall for this area of the state averages 29 inches of rainfall. The average yearly infiltration for the system is therefore estimated at 19.7 Acre-Ft or 6,419,000 gallons. This amount less 1,300,000 estimated gallons used for operations nets the aquifer 5,119,000 gallons of additional water that previously ran off from the site or was used by the crops.

Not taken into account with this calculation are the long linear swales will also allow for good treatment and an extended time of concentration of the water prior to entering the ponding and wetlands.

The existing wetlands have been incorporated into the design and the proposed plan provides for typical bounces within 1-ft of the existing conditions. The proposed grading will maintain the natural

connection between the wetlands as it exists today. With the warm spring this year, our wetland biologist was able to do a site evaluation and locate the boundary of the wetlands on the site. A buffer has been provided surrounding the wetlands and a double row of silt fencing has been incorporated into the plan to prevent any sediment from reaching the wetlands.

The water used for coloration and any excess colored water has been designed to flow back through the coloration pile and be absorbed by the mulch to the north of the coloration equipment. Typical loss for a coloration run is approximately 5 gallons of water, which is easily absorbed by the mulch surrounding the equipment. This off colored mulch is then utilized or colored as black mulch. This equates to a very eco friendly operation.

BUFFERING AND LANDSCAPING

The site is currently designed with a naturalized 8-ft vegetative berm. The sites remaining open areas, including the berm are planned to be seeded with a low grow fescue grass that is adaptable to this soil type and requires no irrigation. The infiltration area is also planned to be vegetated with a defined MnDOT seed mix, appropriate for this type of use. The infiltration area will also be planted with trees that grow well in wet conditions. Landscaping will also be provided for the office building.

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CST COMPANIES, LLC

SITE PLAN NARATIVE

Page 4 of 5

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CST COMPANIES, LLC

CST Distribution, LLC, operating since 1/2011, and CST Transportation, Inc., operating since 2/2012, are owned by Chad & Megan Toft. Chad has worked in the distribution and trucking industries since 1996 and he has been involved in the mulch industry since 1997. Megan has worked in the staffing/ HR industry since 1994. Opening CST Distribution & Transportation has been an easy progression for Chad with his history in the industries and he runs all operations. Megan's background in Human Resources and Finance/Accounting has made it good partnership. She runs all financial/purchasing/HR and customer service operations. 2015 combined Gross Revenues were \$14 million.

CST Distribution

CST Distribution, LLC is a Wholesale Distributor of Softener Salt, Mulch, Ice Melt, Firewood, Washer Fluid and Bottled Water and also a Contract Packager of primarily Mulch and Soils. Our distribution facility is located in Rogers, MN and our bagging facility operates in Elk River, MN.

We serve a diverse customer base, including roughly 2,500 C-Stores, grocery stores, quick lubes, manufacturing plants, car washes, lawn/garden stores and hardware stores. We currently serve Minnesota, Wisconsin, North Dakota, South Dakota, Iowa & Michigan. Some of our larger customers include: SuperAmerica, Holiday Station Stores, Menards, Patio Town, Hedberg, Cub Foods, Home Depot and HyVee.

- 9 FT Employees, 12 Seasonal (March 1st thru Dec 23rd), 1 PT Mechanic
- Office wages: \$45k – 60k, depending on position (Customer Service – Office Manager)
- Mechanic wages: \$20 per hour
- Mulch wages: \$37k – 60k, depending on position (Yard asst. – Shift Supervisor – Yard Foreman)
- Mulch Labor wages: \$12-14 (1st shift) \$14-16 (2nd shift) (Labor – Forklift)
- 1st shift: M-F 7am to 3:30pm with some overtime in the spring loading trucks on Saturday to ship to Home Depot/Menards. Supervisors start at 6:30am (startup machines & daily safety inspections prior to shift start)
- 2nd shift: M-TH 4pm to 2:30am
- Mulch bagging operation (estimated): April 1st thru mid-Nov, weather dependant. August is very quiet as we are waiting on next years contracts. Winter months are used for stocking up on raw material, minimal trucks delivering bulk product.
- Equipment: 1st shift & 2nd shift use payloaders, forklifts and a coloring machine while working. Normally there will be a payloader outside that feeds the hopper with product in the building for the bagging line. Once the pallet is complete a forklift will take this from inside the building to a spot on the N. side of the bagging building. These will go on a trailer, by forklift, and moved to a designated spot in pallet storage. Payloaders will also be used to load/unload bulk mulch trucks and to feed bulk product to the coloring machine. All moving equipment has backup beepers, per OSHA standards. Preferred Sand is

a company mining sand in Woodbury. Due to complaints from residents, within 100 yards of project, once project started they met with OSHA and Mining Organization and were able to switch to a backup light. Flashes when you put in reverse and about the size of a hockey puck. We would be open to installing these if noise was an issue once operating. During 2nd shift only 1 payloader and 1 forklift would be used.

- Noise: Truck traffic, forklifts, payloaders and a coloring machine would be the noise that occurs outdoors. These would happen during hours of operation. There is extremely minimal to no impact on residents. Decibel testing was done with some of City Council members and residents, during peak time and season, and was gauged around 87-89 decibels, during full operation. This is equivalent to loud singing. Normal conversation is 60-65 decibels and that can be done standing next to the coloring machine, bagging machine, payloader etc. Restaurants normally clock at 90 decibels during peak hours. The nearest resident is an estimated 825 Feet away from this equipment. One resident said that "Harleys on 237th are louder than you ever will be." Quaking Aspens on the berm will restrict noise to inside our property. They are used to rustle/ruffle out any noise outside our property. See attachment OSHA.gov, 1910.95(b)(1) & 1910.95(b)(2) We fall under the Permissible Noise Exposure for any personal protective equipment. City Council said no further certified decibel testing is needed after visiting the site, much lower decibel than Hwy 65 and what they were expecting.

- Odors: The main odor we have is a Cedar smell from fresh bulk Cedar. We have no composting mulch and so very limited smell beyond fresh wood smell, within 200 feet of pile. Coloring mulch is odorless. City Council and residents at the site did not complain, mention or smell any odor. Air quality testing was offered and declined by City Council after site visit.

- Mulch Piles: Currently mulch piles are 35-40 feet high at peak. This was measured with City Council members. They are currently at peak height for about 60 days. It takes all winter to build a pile and once production starts in spring it decreases rapidly. We are anticipating lower and longer piles due to having increased acreage and space that we are currently out of, 20-25 feet high, per city code. There is currently no odor as our product doesn't sit long enough in the summer to make a composting smell. Keeping the piles lower and wider will reduce any heat and chance for spontaneous combustion. We have a temperature gauge and monitor 7 feet in to make sure heat isn't becoming excessive. We then would open up the pile to cool it. We haven't ever had this issue since opening our business. Spontaneous Combustion of Mulch happens at 300-400 degrees Fahrenheit. The highest temperature we have ever recorded in one of our piles was 185 degrees F. Average corn silage pile sits at about 250-275 degrees F as it has more vegetation in it, making more heat.

- Fire Lanes: See site plan for fire lanes. We would work with the City of East Bethel Fire Chief to ensure proper and safe fire prevention. Additionally, we have trailer mounted water tanks for movable suppression.

- Office hours: M-F 8am to 5pm with shorter hours in the winter, closing at 4pm.

- Trash: CST will store the trash containers inside the warehouse and roll out when trash will be picked up. We will have incidental office trash and the bagging facility and warehouse will have very minor trash. We recycle all our pallets and reclaim all the mulch we produce.

- Expansion

CST Distribution has grown by 20% yearly, since starting in 2011. We are over capacity at our current facility and can't accommodate any further growth on either the Mulch or Distribution sides. In order to continue the growth with Mulch, acreage is needed to store pallets in the winter and stock additional raw materials in the winter. On the distribution side we have opportunity for expansion of our Softener Salt business but need the additional warehouse space for stocking. We have added one automated bagging machine for Mulch packaging and would like to add a 2nd line for growth, along with a 3rd line for Softener Salt and Ice Melt bagging in the future.

CST Transportation

CST Transportation, Inc. is a Local/Regional Transport Trucking Company, specializing in forklift mounted flatbed trucks, with occasional over the road capabilities. The current location is in Rogers, MN. We currently serve the upper Midwest with occasional over the road to the East Coast. Some of our larger customers include: Cemstone (Twin City Concrete), Morton Salt, North American Salt, Menards, Home Depot, Progressive Rail, Timber Products, Forest Specialties, Patio Town & Pipe Fabricators.

- 4 FT Office Employees, 1 FT Mechanic, 1 FT Warehouse employee, 27 FT Drivers
- Office wages: \$31k - \$55k, depending on position (Dispatch Asst. – Dispatcher)
- Mechanic wages: \$60k – 75k, depending on overtime
- Warehouse wages: \$15 – 16 per hour, forklift operator
- Drivers: \$55k - \$75k depending on driver
- Office hours: M-F 7am to 4pm
- Driver/Mechanic/Warehouse hours: Drivers start leaving as early at 4:30am, depending on delivery times and as late as 8am. They come back anywhere from 2pm to 6pm.
- Equipment: Flatbed trucks are loaded/unloaded with a forklift. We have one warehouse person in this position. Trucks are normally loaded in the afternoon for next day deliveries and he typically leaves between 3-5pm, occasionally later. During the day, we will be unloading wholesale products and putting them in the warehouse. These will be loaded/unloaded at the back of the warehouse. Typical hours on an extremely busy day could range from 6:30am to 6pm and on a very slow day from 8am to 2pm. Forklifts have backup beepers, could be changed out to backup light, if noise becomes an issue.
- Expansion
 - CST Transportation has been adding 3-4 trucks per year, since opening in 2012. We are over capacity at our current facility and can't accommodate any further growth due to lack of space for truck & trailer parking. Not all 27 trucks park at our facility, some drivers keep at rented spaces around metro (closer to their residence) or at their residence if meets city requirements. Currently only 10 park at our facility during the winter, could be up to 14 at new location. We usually have 3-6 trucks during the week in the summer and all 10 on the weekends to get washed. Most drivers keep them at home during the spring/summer/fall due to not having to plug in during the winter.

Summary

- 42 Full-Time employees, 12 Seasonal employees, 1 Part-time employee
 1. Main building – 11 full time employees
 2. 2nd building – 4 full time employees, per shift (this is less as we continue to switch to fully automated)
 3. Other employees are working in the field (drivers)
- Wages range from \$31k - \$75k, depending on position
- Hours of operation: vary depending on position and time of year
- There are about 40 trucks, on average, in and out per day. (see Site Plan for routes and circulation)
- Expansion plans:
 1. Mulch growth of 10-15% per year in prior years
 2. Opportunity to grow palletized Softener Salt business if space is acquired
 3. Opportunity for additional bagging capabilities if space is acquired
 4. Potential Retail Center located where old house is
 5. Would need 2nd location with rail access
 6. Prior years 3-4 drivers were added, 2 were added in 2016
 7. Add 1 Salesperson and 1-2 Customer service employees
 8. Add 1 Full-time 2nd shift Mechanic and 1-2 Warehouse Forklift drivers

9. Would like to acquire 20 acres to the North for future expansion

- Screening: Berm running the perimeter of 237th & behind Coopers Corner - see attached site drawing
- Runoff: 2 ponds built for runoff – see attached rough site drawing
- Right of way on Davenport: CST is ok with Right of way on Davenport for future road, 80 ft. easement

Timeline

- Property: Purchase agreement is finished and will close upon final approval from City of East Bethel
- Construction: Would commence immediately upon final approval from City of East Bethel
- Partial Occupancy needed by 8/1/16, Bagging facility must move by then as there is only one chance to move per year. We start bagging for next season during the month of August.
- 2nd building is estimated to be done at the same time or within 30 days. Total move by 11/1/2016 at latest.

Building Materials

- The building structures will be a steel frame with concrete footings and masonry foundation.
- The base material will be a 4'-0" high rockface block with masonry sill material from North Star Masonry Slate Gray color on both buildings
- The remainder of the exterior wall will be a 3" x 3'-4" wide insulated galvanized prefinished heavy embossed panel installed vertically. The color to be Surrey Biege. These panels give the building a stucco look with an R value of 24.
These panels are used on many buildings throughout the Twin City Metro Area. They have been install on the Millenium Hotel, Mpls., 3M Buildings, Cottage Grove, BAE Consolidation, Mpls., and Portland Towers, Mpls.
- The office area will be two colored EFIS to match the Burnished Brown and Surrey Biege colors above the 4'-0" high rockface block wainscot.
- The roof on the office area will be a Nucor Standing seam roof in the Burnished Brown color
- The roof on warehouse and Bagging Facility to be a ribbed roof panel in Fox Gray color
- The metal trims and soffits to ne a prefinished metal in Aztec Blue.
- The exterior Doors and windows will be a brown to match the Burnished Brown

Locke
SUPPLY CO.
An Employee Owned Company

PLUMBING



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Are mulch colorants safe?



Summary & Conclusion

Current investigations indicate that mulch colorants pose no threat to people, pets or the environment. The dyes currently used by the mulch and soil industry are similar to those used in the cosmetic and other manufacturing industries (i.e., iron oxide) and pose no health or environmental risk.

Background

In the early 2000's, occasional media reports surfaced that questioned the human safety of colored mulches. The MSC investigated these claims, undertaking its own detailed studies and literature search. A Colorant Committee was formed in late 2006 and meetings were held throughout 2007.

Mission and Goal

The mission of the committee was to document information on existing or emerging colorants and their components to address any concerns for human health and environmental safety by the public, regulators and media. The goal of the committee was to determine if standards are necessary for certified colored mulches.

Investigational Results

Below is a brief summary of the committee's investigational findings:

a. Human Safety of Colorants

Iron Oxide (Red):

Iron Oxide is used extensively in facial cosmetics, paints, and other chemicals. It has been used for centuries. Following an extensive review of national and international literature and information, no specific concerns were identified.

Carbon Black:

Carbon black is virtually pure elemental carbon and is used in many consumer and industrial products such as tires, belts, virtually all other rubber goods, video and audiotapes, nearly all electric motors as the brush contacts, insulators, and dry cell batteries. As a pigment, it is used as a toner for paper copiers and printers, inks for newspaper, and in most dark-colored paints and coatings.

Given the wide manufacture of both pure carbon black and products containing carbon black, there is a wealth of information published on the human health aspects of this material. Occupational studies over 60 years do not show any increased health risk to workers exposed to carbon black compared to the general public.

The US EPA also concluded that no special considerations are necessary for carbon black as it pertains to exposure for infants and children. There are no adverse effects for acute toxicity in humans and animal studies. It is considered non-toxic. Also, there were no problems with skin sensitization or eye irritation (beyond a simple physical irritant).

b. Environmental fate

The committee did not find any evidence that any of the components of colorants were an

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environmental concern when used according to label directions and rates on mulch.

Committee Conclusions

The committee concluded that based on the available toxicological and environmental profiles of the ingredients commonly used in mulch colorants, there appears to be no significant areas of concern in using these materials in general, and in certified mulches.

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MPCA Air Emission Values

From: Le, Hien (MPCA) <hien.le@state.mn.us>

Date: Tuesday, April 19, 2016

Subject: Air Emissions Calculator

To: Todd Erickson <todd@tericksonllc.com>

Hi Todd,

I believe you are on the right track to come up with the emission values. The permitting threshold for total particulate matter (PM) is 100 tons per year. Therefore, your number is well below the required Air permit level.

However, please note my words are only served as recommendations. They are not official determinations.

Glad I can help. Let me know if any concerns or questions!

-Hien

Hien (Ian) Le

Small Business Environmental Assistance Program (SBEAP)

Minnesota Pollution Control Agency (MPCA)

520 Lafayette Road N.

St. Paul, MN 55155

651-757-2519

Hotline: 651-282-6143 or 800.657.3938

hien.le@state.mn.us

www.pca.state.mn.us/sbeap

CST Air Emission Calculation - East Bethel

MPCA - Fugitive Emission Spreadsheet

4/19/2016 / All Haul Routes Included

Totals					
a	b	c	d	e	f
Source	Emission Factor	Potential Activity	Potential Emissions	Actual Activity	Actual Emissions
			$b * c / 2000$		$b * e / 2000$
			(ton/year)		(ton/year)
Unpaved road	(lb/VMT)	(Vehicle miles traveled)		(Vehicle miles traveled)	
PM	8.16	2949	12.03	2003.84	8.17
PM10	2.17	2949	3.21	2003.84	2.18
Material handling	(lb/ton)	(tons)		(tons)	
PM	0.01	39,000	0.11	26500.00	0.11
PM10	0.00	39,000	0.05	26500.00	0.05
Ground pile	(lb/d*acre)	(d*acre)		(d*acre)	
PM	14.67	912.5	6.69	912.50	6.69
PM10	7.33	912.5	3.35	912.50	3.35
Total					
PM			18.83		14.97
PM10			6.60		5.57

Note: 1) Threshold for MPCA Emission Permit is 100 Tons per Year. Total PM for CST is max 12 Tons Per Year (MPCA Contact: Hien Le 651-757-2519)

2) Potential Emissions Represents 150% of current production.

CST Companies, LLC

Dust Control Plan

Mulch Processing and Distribution Facility

237th Avenue, East Bethel, MN

Introduction

CST Transportation, Inc. and CST Distribution, LLC (collectively “CST”) are proposing to construct and operate a light industrial mulch processing facility on approximately 40 acres in East Bethel, Minnesota. While the facility is being designed to minimize fugitive dust relating to normal operations, this Dust Control Plan (“Plan”) will be used to address dust concerns as they arise.

This Plan has been developed to control fugitive dust from storage of mulch outdoors, material handling operations, truck loading and distribution and employee vehicle traffic. All mulch inventory is ground-up offsite and delivered to the facility by vehicles owned by CST. All bagging operations are done inside CST’s building. Bagged and palletized inventory is stored outside in designated areas before being distributed to CST customer locations.

CST will regularly observe external operations at its facility to ensure no unusual conditions exist that will lead to fugitive dust. As conditions warrant, or upon receipt of a verified complaint, CST will apply water to its parking lot and drive aisles to limit any migration of dust from regular operations. CST will maintain a record of complaints received and its response to such complaints, including whether the dust conditions warranted application of water controls.

CST’s processing and distribution activities operate at peak capacity from March to July in a given year, in conjunction with normal commercial and residential landscaping seasons for the upper Midwest. Mulch inventory is stockpiled during winter months.

This Plan is prepared based on best practices applicable to CST’s business operations and the anticipated dust control measures likely to be required to manage fugitive dust. Questions about this Plan can be directed to:

Chad Toft
CST Companies, LLC
23805 Highway 65 NE
East Bethel, Minnesota
763-515-6660

Possible Dust Sources

Sources of dust arising from CST's processing and distribution of bagged mulch may include:

- Outdoor storage of mulch
- Material handling and transfer
- Equipment operations on unpaved surfaces
- Truck and employee vehicle traffic

CST will have pre-ground mulch delivered to its facility in trucks. Stockpiles of ground mulch will be placed at the facility on paved surfaces in designated locations on the west side of the Site based on the approved Site Plan. Drive aisles will be maintained around the stockpiles to facilitate easy movement of mulch by payloaders into the CST building for bagging operations.

Bagged and palletized mulch will be returned to designated outdoor storage areas on the east side of the Site to await shipment via CST trucks to customer locations. All processed mulch is fully enclosed and secured from any wind disturbance while awaiting wholesale customer shipments.

Palletized mulch will be loaded onto CST trucks for delivery to wholesale customers. Approximately 40 trucks will exit daily from the facility during the morning hours, returning to the facility in the late afternoon hours.

Dust Control Measures

It is not anticipated that normal operations and weather conditions will require application of dust control measures. However, when weather conditions, such as high wind conditions, create the potential for fugitive dust from the facility, CST will apply water to all drive lanes around the mulch stockpiles and palletized inventory.

CST will actively monitor dust control measures to ensure appropriate steps are being taken to minimize fugitive dust when it becomes necessary.

4814-4892-4976, v. 1

CST Companies
East Bethel, Minnesota

East Bethel Facility
Noise Assessment

Prepared for
CST Companies

by

David Braslau Associates, Inc.

20 April 2016

EXECUTIVE SUMMARY

This noise assessment has been prepared in response to the proposed CST Companies mulching facility in East Bethel, Minnesota, which will be located close to residential land uses. The objectives of this study are to establish realistic estimates of sound associated with mulching operations and to compare them with state noise standards and existing ambient sound levels.

Sound levels were monitored at the CST facility in Elk River, Minnesota, primarily in an area of most outdoor activity between mulch piles, the coloring machine, and the hopper feeding the bagging structure. Most noise is associated within moving front loaders that move material to the coloring machine or the hopper. Smaller bobcats and lifts also operate on the site but will mostly be contained in a proposed new building.

Based upon monitored levels, L50 or median sound levels from this activity at the new site were predicted at adjacent homes and compared with the state L50 daytime and nighttime standards. The levels were found to be well below the standards.

On-site noise levels from the storages areas at the new site were estimated and found to be generally below 40 dBA is rarely reached because of roadway traffic noise.

Finally, on-site facility levels combined with truck traffic noise associated with the facility were compared with existing ambient levels associated with traffic on TH 65 and 237th Ave N. The theoretical increase in the existing ambient level was predicted to be generally less than 1 dBA. While noise from the proposed facility might be heard, it will likely not be different from other sounds in the area associated with traffic and other activities.

Based upon these findings, while there might be some limited increase in sound level, the proposed facility will have minimal impact related to state noise standard and existing ambient levels.

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1.0 INTRODUCTION

1.1 Study Description

This *Noise Assessment* evaluates potential noise impacts on residential land uses adjacent to the proposed CST Companies East Bethel facility east of Trunk Highway (TH) 65 and north of 237th Avenue N in East Bethel, Minnesota.

The study is based upon sound level monitoring of an existing facility in Elk River, Minnesota, establishing facility sound sources, projecting sound levels to the adjacent residential land uses. Expected sound levels are compared with Minnesota state noise standards for residential land uses and with ambient sound levels in the area due to nearby roadways.

Location of the proposed facility relative to adjacent homes in East Bethel is shown on Figure 1.1.

1.2 Study Objectives

The objective of the study is to determine potential noise impacts on residential land uses (NAC-1) relative to state noise standards in Minnesota Rules 7030.0040 as noted in **Table 1.1**

Table 1.1 Minnesota Noise Standards (Mn. Rules 7070.0040)

Noise Area Classification	Daytime		Nighttime	
	L ₅₀	L ₁₀	L ₅₀	L ₁₀
NAC-1 (residential and sensitive areas)	60	65	50	55
NAC-2 (commercial)	65	70	65	70
NAC-3 (industrial)	75	80	75	80

The L50 refers to levels that occur more than 50% of an hour while the L10 refers to levels that occur more than 10% of an hour. The daytime period includes hours between 7:00 am and 10:00 pm. The nighttime period includes hours between 10:00 pm and 7:00 am.

A secondary objective is to compare estimated facility sound levels with existing ambient sound levels in the area associated primarily with traffic on nearby roadways.

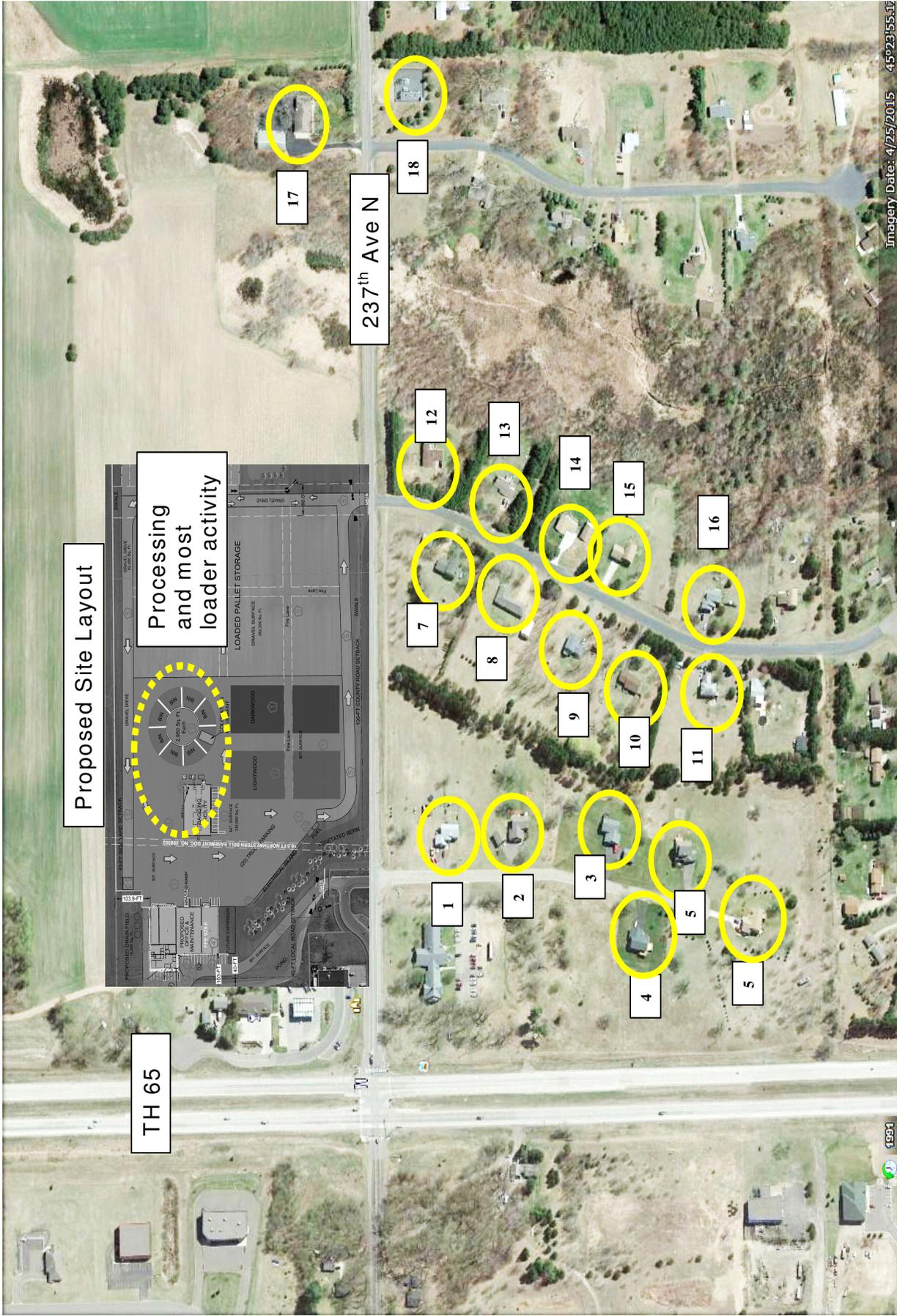


Figure 1.1 Location of the Facility and Adjacent Residential Land Uses

2.0 SOUND LEVEL MONITORING

2.1. Monitoring in Elk River on Monday, 18 April 2016

Sound level readings were taken close to typical yard operations using a Larson-Davis Model 820 Type 1 sound level meter (S/N 1706), which collected overall A-Weighted (dBA) levels every second and was located approximately 85 feet from the coloring machine and 100 feet from front loader activity. A Larson-Davis Model 824 Type 1 sound level meter (S/N 1338) collected one-third octave band sound level spot readings close to the front end loader, which was the primary sound source. This provided a basis for establishing detailed sound source information for use in predicting sound levels at the East Bethel site. The meters were calibrated before and after the readings with a Larson Davis Model CA250 calibrator (S/N 2122). Location of the stationary LD820 meter is shown on **Figure 2.1**. Meteorological conditions were ideal with dry and almost calm conditions.

Selected photographs of the operations monitored are included in **Appendix A**. Several videos of operations by the meter were also taken and can be made available upon request, since the files are too large to transmit via the Internet.

2.2. Results of Continuous Sound Level Monitoring

The dBA 1-second time history of operations is included here as **Figure 2.2**. The steady state sound source was due primarily to the coloring machine while peaks represent passing of a front-end load at a distance of approximately 20 feet. One-minute statistical levels were also recorded and are shown on **Figure 2.3**. These permitted an accurate representation of sound levels from the East Bethel sites. Average hourly values for statistical descriptors were L10 64 dBA and L50 62 dBA. Most of the monitored activity occurred within about 100 feet of the meter so this was assumed as a conservative source distance for modeling.

2.3. Results of Spectral Spot Readings

Spot readings were taken when the loader was operating (moving or lifting) at a distance of approximately 20 feet from the meter. The representative sound level spectrum extracted for a front-loader at 20 feet is shown on **Figure 2.4**.

1-second Time History

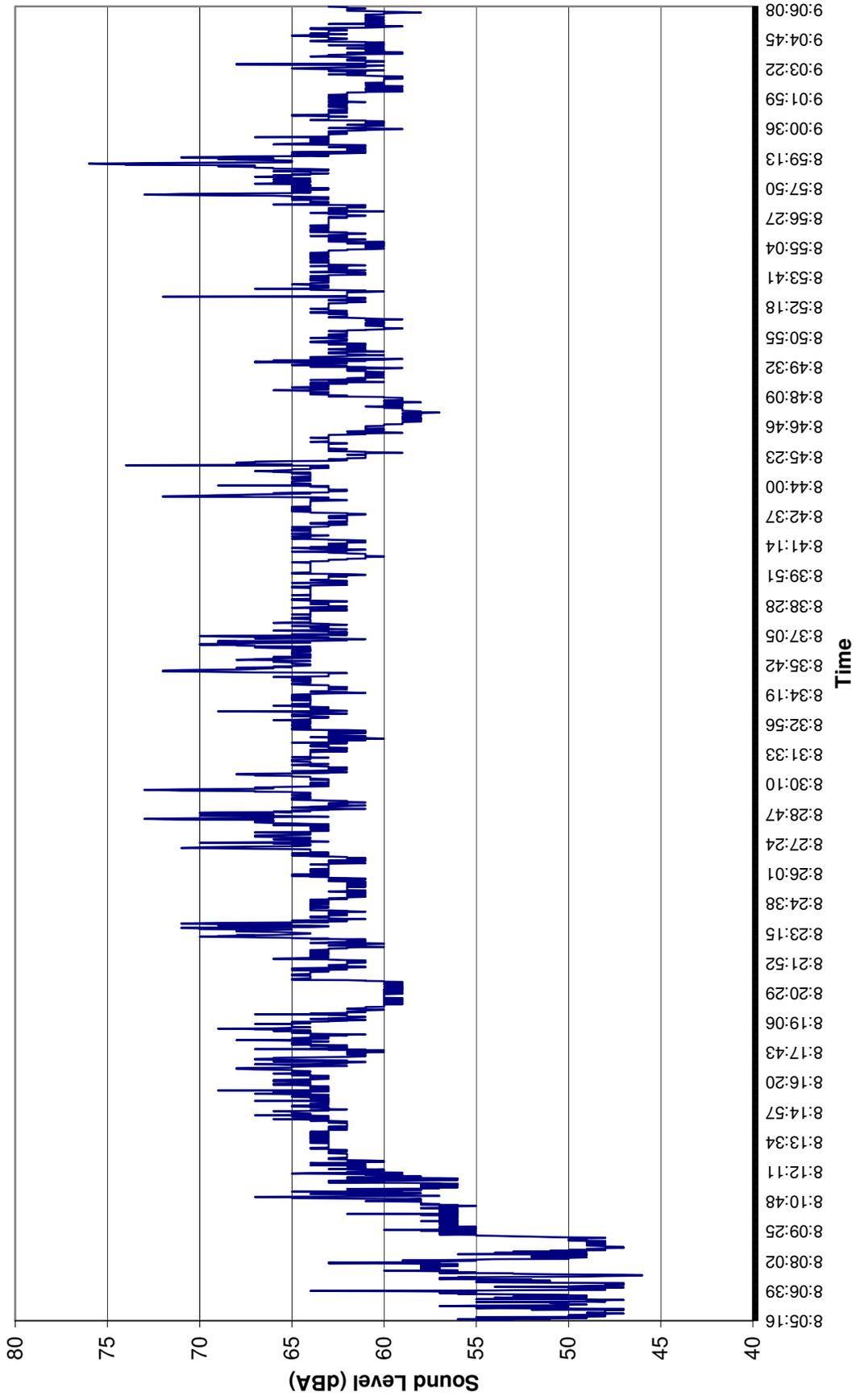


Figure 2.2 One-Second Time History

One-Minute Intervals

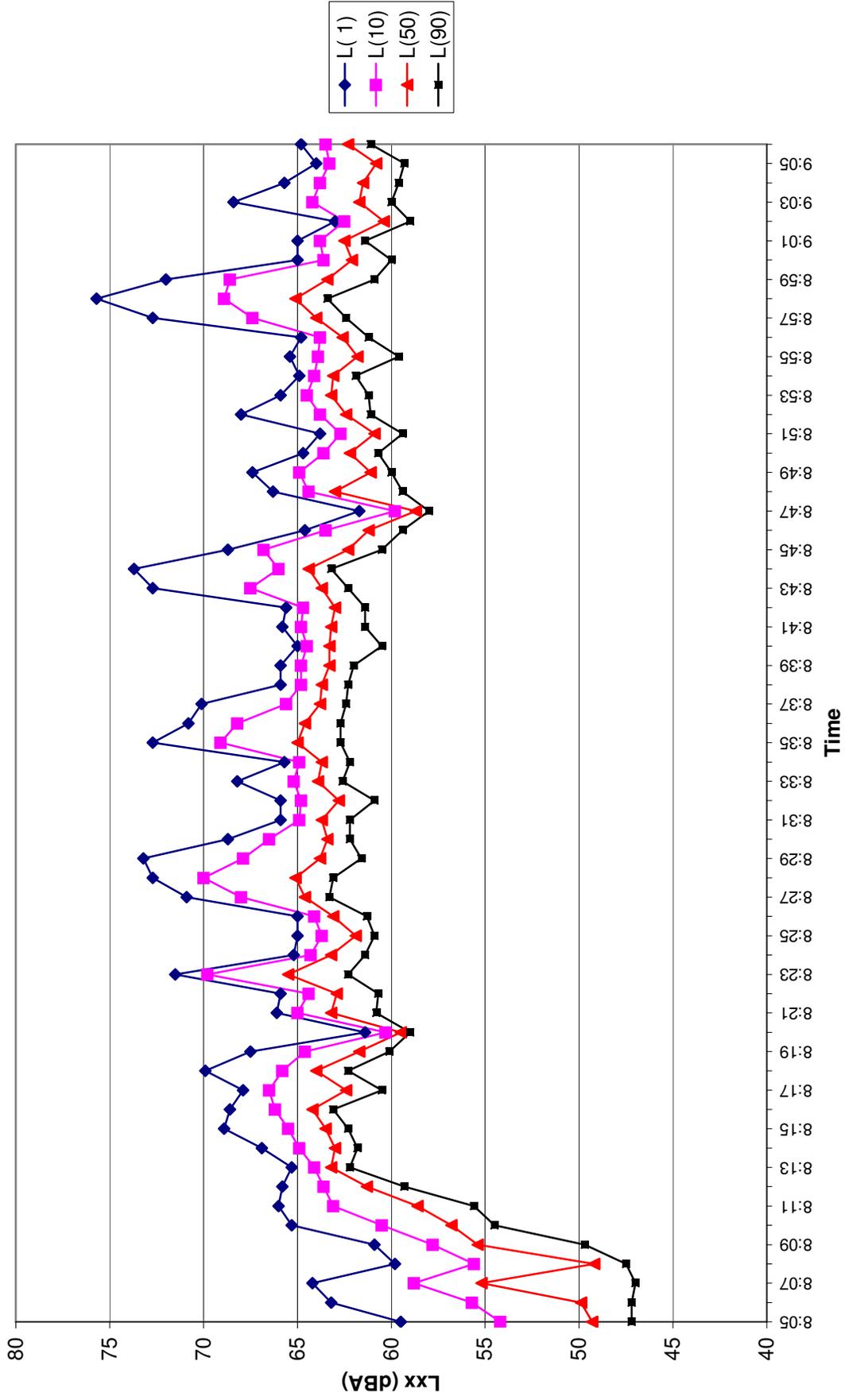


Figure 2.3 One-Minute Statistical Levels

Assumed Loader Source Spectra (20 ft)

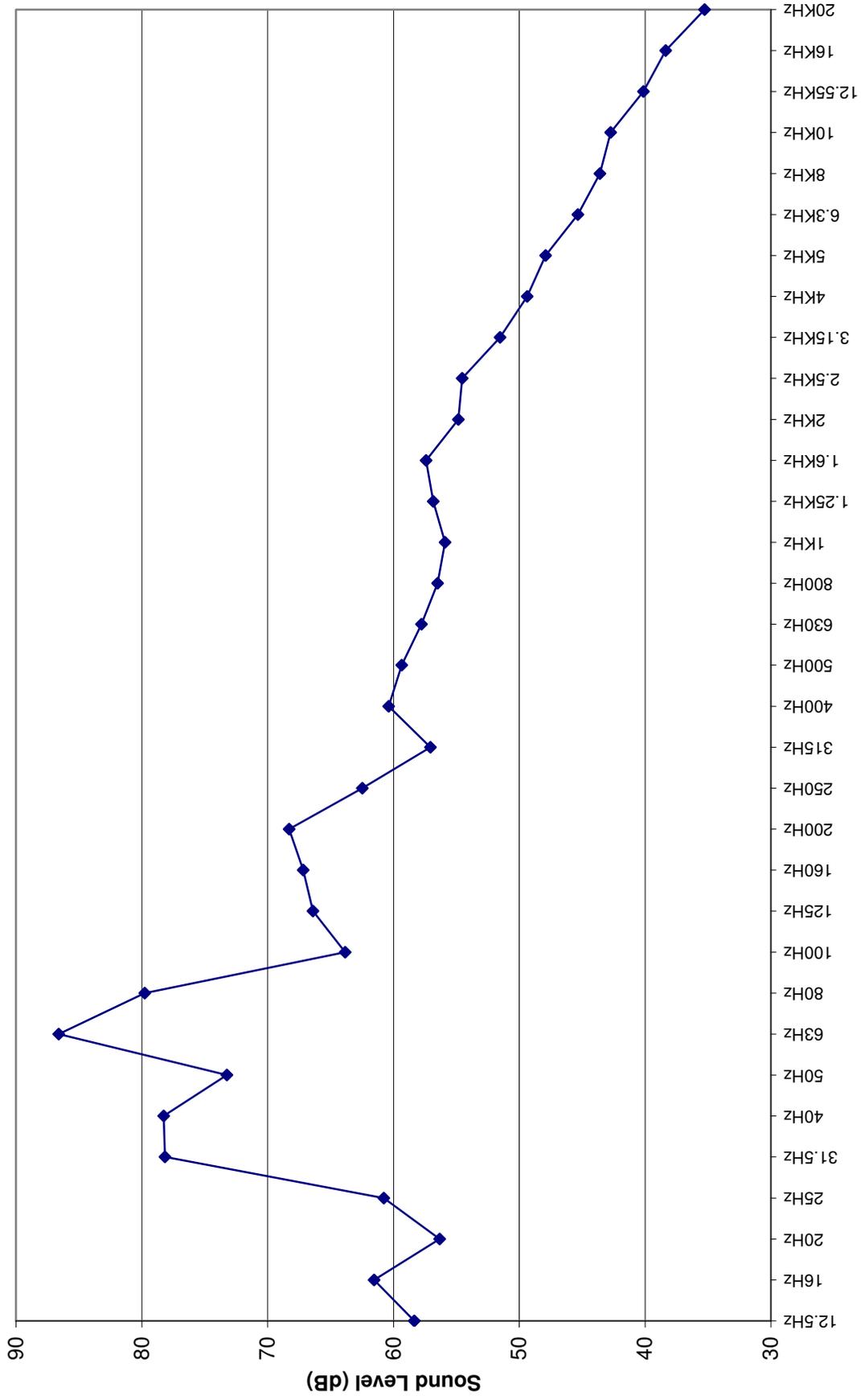


Figure 2.4 Average Loader Spectrum at 20 ft

3.0 SOUND LEVEL PREDICTIONS

The most restrictive state standard is the L50, or level exceeded for 30 minutes of an hour. As noted in Section 2.2, the estimated “source L10” is 64 while the estimated “source L50” is 62, or just two dBA lower. Since the L10 standard is 5 dBA higher than the L50 standard, the L50 source is more critical.

For modeling of loader sound levels within the “activity” area shown on **Figure 1.1**, the representative spectrum has been adjusted to the 62 dBA source level.

Predicted L50 levels at the homes south of the proposed East Bethel site are presented in **Table 3.1** and compared with the daytime and nighttime state L50 standards.

Table 3.1 Predicted L50 Levels at the Nearest Homes

Home	Predicted L50	Day L50	Exceedance	Night L50	Exceedance
1	42	60	-18	50	-8
2	39	60	-21	50	-11
3	37	60	-23	50	-13
4	36	60	-24	50	-14
5	35	60	-25	50	-15
6	34	60	-26	50	-16
7	40	60	-20	50	-10
8	39	60	-21	50	-11
9	38	60	-22	50	-12
10	36	60	-24	50	-14
11	35	60	-25	50	-15
12	38	60	-22	50	-12
13	38	60	-22	50	-12
14	37	60	-23	50	-13
15	36	60	-24	50	-14
16	34	60	-26	50	-16
17	34	60	-26	50	-16
18	33	60	-27	50	-17

It can be seen from the table that estimated sound levels from loaders moving in the activity area shown on Figure 1.1 are well below the daytime and nighttime standards, although activity at the level assumed here is not likely to occur during the nighttime hours.

Predicted maximum sound levels at the three closest homes to a loader moving around the storage area at representative locations shown on **Figure 3.1** are shown in the chart on **Figure 3.2**. It can be seen that the level from most locations are below 40 dBA.



Figure 3.1 Layout for Distributed Loader Sound sources

Predicted Peak Loader Level in Storage Area

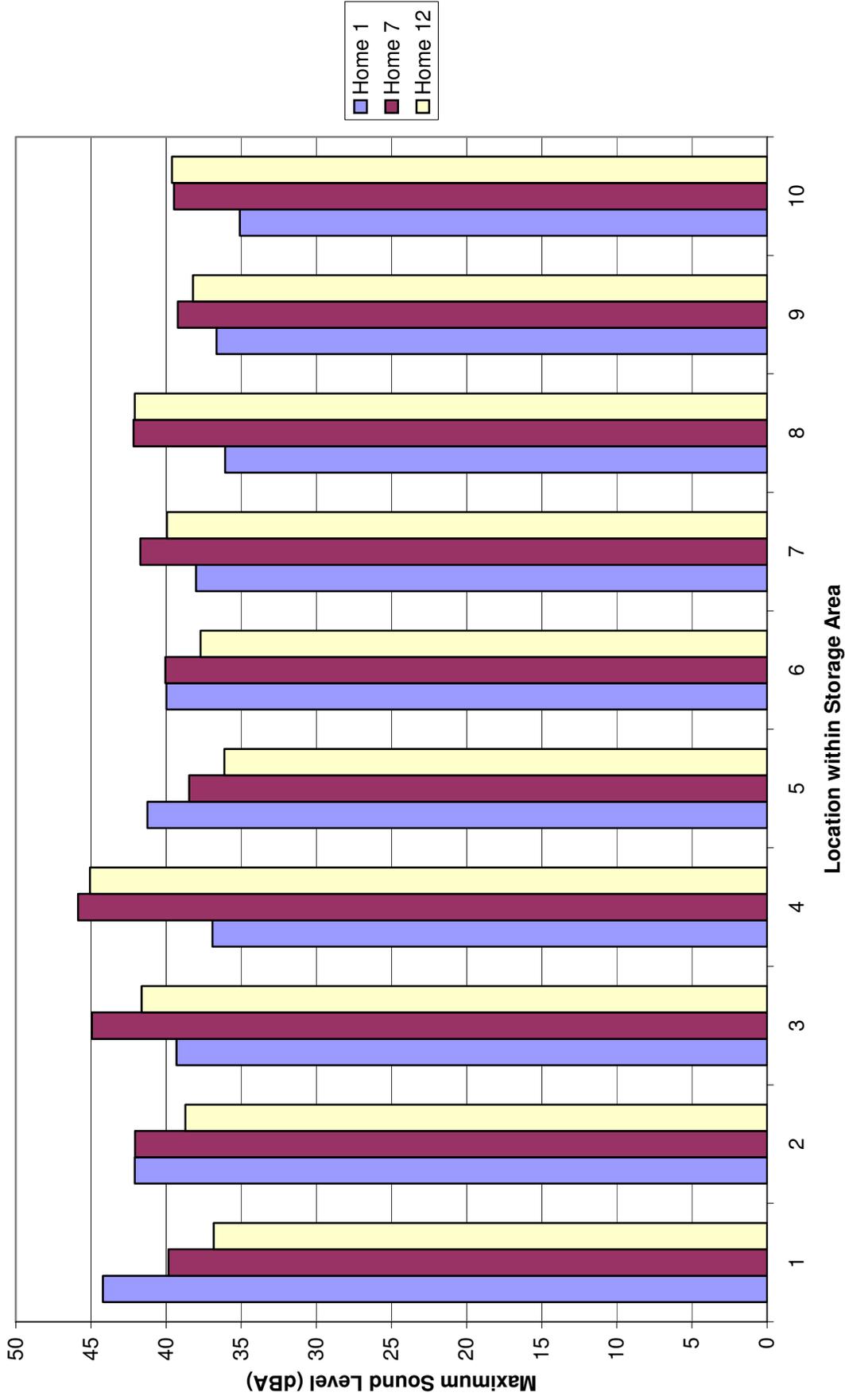


Figure 3.2 Maximum Loader Sound Level in Storage Area

4.0 IMPACT ON AMBIENT SOUND LEVELS

Some ambient sound levels in the residential area can be associated with traffic along TH 65 and 237th Avenue North. Other sources may contribute as well, but this analysis is limited to levels that can be realistically modeled from roadway traffic data.

4.1. Noise Levels form TH 65

Average hourly traffic along TH 65 just south of East Bethel extracted from MnDOT ATR 365 hourly data is shown on **Figure 4.1**. For purposes of estimating ambient sound level at the homes shown on **Figure 1.1** associated with moving traffic along TH 65, three different time periods have been selected” 7-8 am, 12-1 pm (12-13) and 4-5 pm (16-17).

The MinnNoise traffic noise model was used to estimate the L50 sound level for comparison with the predicted L50 level associated with the proposed CST operation. For this study a speed of 65 mph has been assumed which is the posted speed. The assumed vehicle mix based upon other studies in Minnesota, are shown in **Table 4.1**.

Table 4.1 Assumed Vehicle Mix on TH 65

Hour	Percent by Vehicle Type		
	7-8 am	12-1 pm	4-5 pm
Cars	92	94	95
Med Truck	3	2	2
Hvy Trucks	5	4	3

Predicted L50 levels at the homes on **Figure 1.1** are shown below in **Table 4.2**.

Table 4.2 Predicted TH 65 Traffic L50 Levels

Home	7-8 am	12-1 pm	4-5 pm
1	59	57	60
2	59	57	60
3	59	57	60
4	63	61	64
5	60	58	60
6	62	60	63
7	53	51	53
8	54	52	54
9	55	53	55
10	55	53	56
11	55	53	55
12	51	49	52
13	52	50	52
14	52	50	53
15	53	51	53
16	53	51	54
17	47	45	47
18	46	44	47

4.2. Noise Levels from 237th Ave. N

Traffic volumes and speeds on 237th Ave. N are well below the 24,000 ADT on TH 65. The MnDOT traffic flow map for 2014 indicated an ADT of 3,700 vehicles on this roadway just east of TH 65.

Two alternative noise scenarios have been analyzed for this roadway – existing and with limited truck traffic associated with the CST operation. An estimated 40 trucks per day in and 40 trucks per day out of the proposed facility would generate 80 trucks per day along 237th Ave. N between TH65 and the facility entrance east of TH 65. While not directly governed by state noise standards, this traffic along with other sound levels from the facility was used here to evaluate the potential impacts on ambient noise levels.

It has been assumed that a typical daytime hour on 237th Ave. N would carry the vehicle mix shown in Table 4.3. With CST the number of trucks would increase by 10 trucks per hour. A speed of 40 mph has been assumed on this section of roadway just east of TH 65, although the posted speed is 55 mph.

Table 4.3 Assumed Vehicle Mix on 237th Ave. N.

	Existing	With CST
CARS	213	213
MT	7	7
HT	2	12

Predicted results of this analysis are presented in **Table 4.4**. The table is a bit busy and needs some explanations. The first three columns show existing levels on the two roadway and combined levels from both. The next three columns show levels from 237th Ave N, with CST truck, the above predicted on-site CST levels and the total level experienced by each home. Combining this with levels from TH 65 would be the total experienced by the homes with CST. The last column is the expected change over existing levels with the CST which are generally less than 1 dBA.

Table 4.4 Results of Ambient Noise Impact Analysis

Home	237th Ave No CST	TH65 Noon	Both Roadways Existing	237th Ave w/CST	CST Onsite	with CST	TH65 Noon	All Souces	Increase with CST
1	44	57	57	48	42	48	57	58	0.3
2	41	57	57	44	39	45	57	57	0.2
3	37	57	57	39	37	41	57	57	0.1
4	35	61	61	38	36	40	61	61	0.0
5	34	58	58	37	35	39	58	58	0.0
6	33	60	60	35	34	38	60	60	0.0
7	45	51	52	48	40	49	51	53	1.1
8	41	52	52	44	39	45	52	53	0.5
9	39	53	53	42	38	43	53	53	0.3
10	37	53	53	39	36	41	53	53	0.2
11	34	53	53	37	35	39	53	53	0.1
12	46	49	51	50	38	50	49	53	1.7
13	41	50	50	44	38	45	50	51	0.7
14	39	50	51	42	37	43	50	51	0.4
15	37	51	51	40	36	41	51	51	0.3
16	35	51	51	37	34	39	51	51	0.2
17	42	45	47	43	34	44	45	47	0.7
18	45	44	48	45	33	46	44	48	0.1

TH 65 - Average Weekday Traffic - May 2015

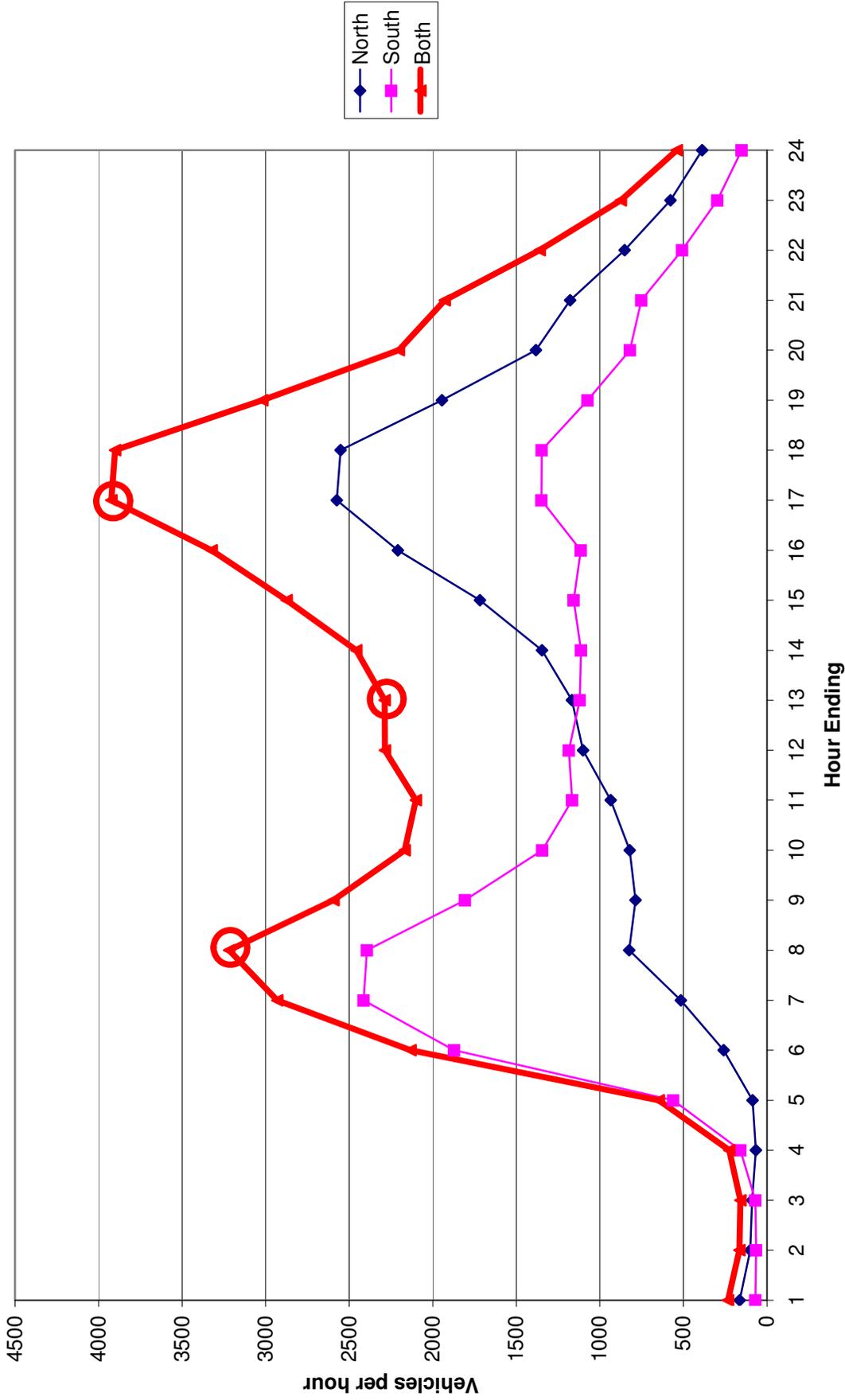


Figure 4.1 TH 65 Average Hourly Weekday Traffic – May 2015

5.0 SUMMARY OF FINDINGS

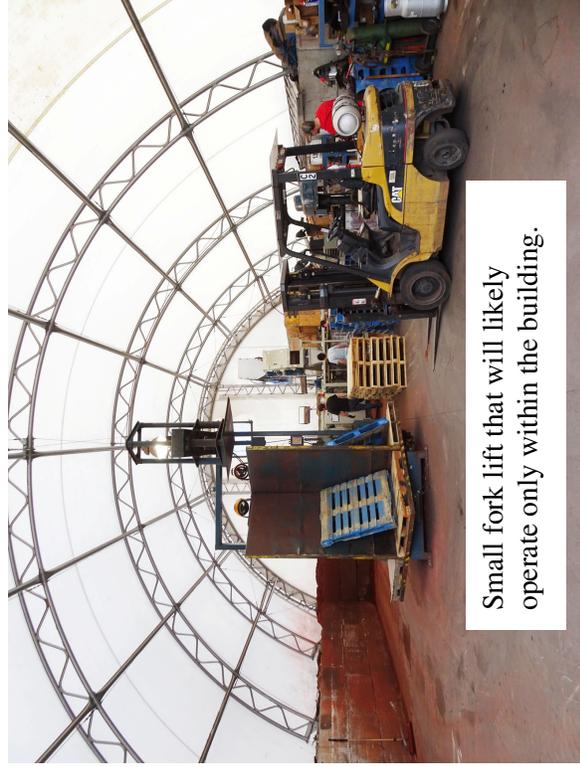
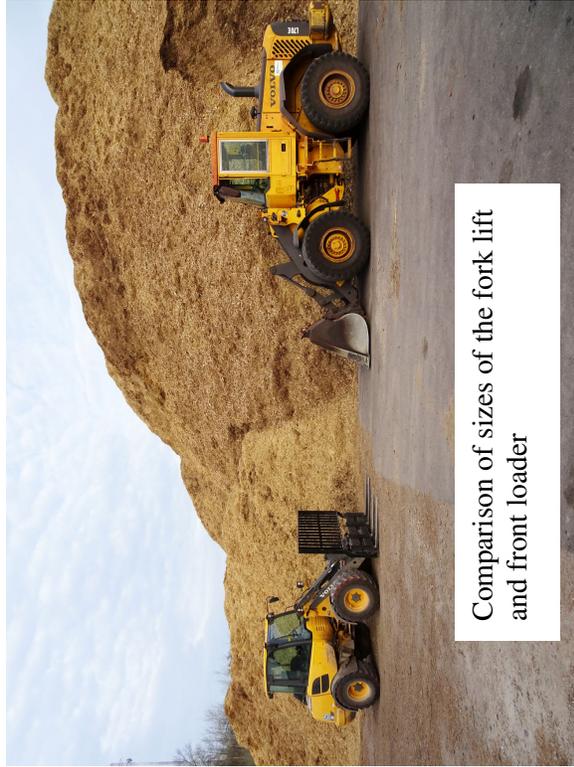
From **Table 3.1** comparing estimated L50 levels at the homes south of 237th Ave. N. from outdoor activities (primarily front load operations) with Minnesota noise standards for residential land uses, it can be seen that these are well below the state daytime and nighttime noise standards. These levels are based on monitored sound levels of actual operations at the CST facility in Elk River, Minnesota, and projected using standard procedures contained International Standard 9613-2 on outdoor sound propagation.

The observed sound level from the Volvo loaders being used at the existing site were considerably lower than levels typically monitored in mining operations. With a peak level of 75 dBA at 20 feet, the loaders being used at CST, are 10 to 15 dBA lower than many we have encountered. Although the loaders are constantly moving, they do not use backup beepers which can be annoying to adjacent properties. At least one Bobcat was observed using a backup beeper. It is recommended that any equipment operated outside of an enclosed building be equipped with broadband backup alarms that can be heard by employees but merge with background noise at a distance.

From Table 4.4 comparing the expected sound level environment with the CST operation and existing ambient sound levels, it can be seen that the expected increase is generally less than 1 dBA. These predictions were based upon a number of assumptions, although our experience with traffic noise and the MinnNoise traffic noise model show reasonable agreement with actually monitored traffic noise levels. Although theoretical predictions of L50 (which is the median sound level) show very little change in the overall ambient level with the CST facility, some sound from the storage area closest to 237th Ave. N. might still be heard.

Much of the activity will be contained in an enclosed bagging building and some screening from a berm along 237th Ave. N. could provide some additional shielding that has not been assumed in this assessment.

APPENDIX A
SELECTED PHOTOS

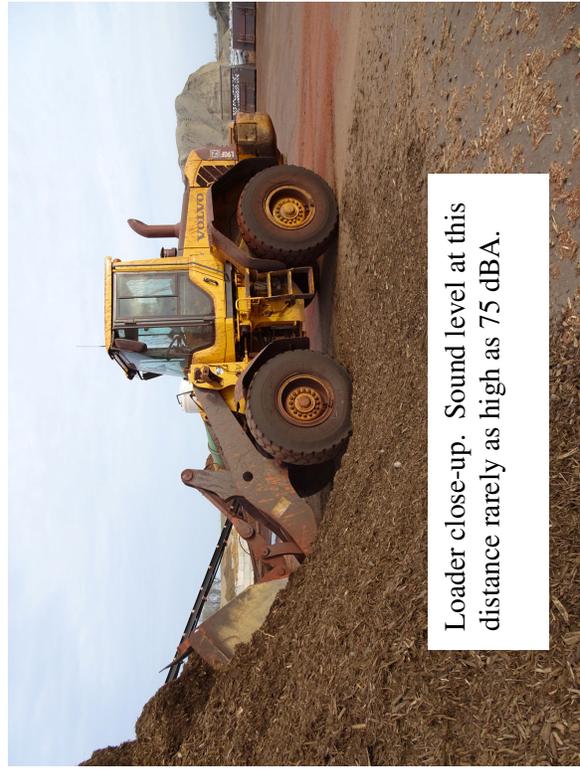




Loader at the coloring machine
--- but notice another immediately left.



Dumping mulch in a hopper for the coloring machine.



Loader close-up. Sound level at this distance rarely as high as 75 dBA.



View of the coloring machine (green rotating cylinder) -- camera for video at right.



Anoka County

TRANSPORTATION DIVISION

Highway

Douglas W. Fischer, PE
County Engineer

April 21, 2016

Colleen Winter
City of East Bethel
2241 221st Ave. NE
East Bethel, MN 55011

Dear Colleen,

We have reviewed the site plan for CST Companies, LLC to be located east of Baltimore Street NE and north of CSAH 24 (237th Avenue NE) within the City of East Bethel, and I offer the following comments:

The existing right of way along this portion of CSAH 24 is 60 feet which should be adequate for future reconstruction purposes. Consequently, no additional right of way will be required at this time.

It appears that clearing will be required to meet the Case I intersection sight distance requirements at the Davenport Street NE alignment access point. Care must be exercised when locating signs, buildings, structures, plants, berms, etc. outside of the county right of way, so as not to create any new sight obstructions for this development.

Local roadway intersections on the county highway system are to be spaced utilizing engineering judgement, in accordance with the current ACHD Access Spacing Guidelines. Whenever possible, commercial developments should obtain cross easements with adjacent commercial properties or create/construct frontage and backage roads so as to minimize the proliferation of access points on the county highway system. This development will increase turning maneuvers on CSAH 24, which is a safety concern. Consequently, as part of the ACHD Engineering Plan Review and Permit processes, right and left turn lane construction on CSAH 24 at the Davenport Street NE alignment access point will be required to be completed in conjunction with this development. Turning maneuvers are also anticipated to increase at the Baltimore Street NE alignment access point as well following this development, and the City may wish to consider the construction of turn lanes on CSAH 24 at this access point as well.

The ACHD Engineering Plan Review process will apply to this development. Calculations must be submitted along with a grading and erosion control plan that delineates the drainage areas for this development. The post-developed rate/volume of runoff must not exceed the pre-developed rate/volume of runoff for the 10-year, critical design storm. Contact Zachary Borgerding, Engineer I, via telephone at 763.862.4263, or via e-mail at Zachary.Borgerding@co.anoka.mn.us

Our passion is your safe way home!

1440 Bunker Lake Blvd. NW ▲ Andover, MN 55304-4005
Office: 763-862-4200 ▲ Fax: 763-862-4201 ▲ www.anokacounty.us/highway

Affirmative Action / Equal Opportunity Employer

for further information and to coordinate the ACHD Engineering Plan Review process. Please submit the drainage calculations, grading and erosion control plans (including culverts with 6:1 safety grates and aprons in the county right of way), the ACHD Design Requirements Checklist for County Highway Modifications (copy available via our website), and ACHD Engineering Plan Review fee (currently estimated at \$550.00) to Mr. Borgerding for his review and approval.

Following completion of the ACHD Engineering Plan Review process, the ACHD Permit process can begin. A Commercial Access permit and a permit to work within the Anoka County ROW are required and must be obtained by the contractor(s) prior to the commencement of construction. License Permit Bonding, methods of construction, design details, work zone traffic control, restoration requirements and follow-up inspections are typical elements of the permitting process. Please contact the ACHD Permit office at 763.892.4224 for further information regarding the permit process.

Thank you for the opportunity to comment. Feel free to contact me if you have any questions regarding this review.

Sincerely,

A handwritten signature in black ink, appearing to read 'Jane Rose', with a long horizontal flourish extending to the right.

Jane Rose
Traffic Engineering Manager

Xc: File - CSAH 24/Plats + Developments/2016
Larry Hoium, County Surveyor
Randy Bettinger, Traffic Engineering Coordinator
Josie Scott, Traffic Engineering Technician
Permit Office
Zachary Borgerding, Engineer I

Animal, Kennel

Case Number	Address	Owner	Violation	Property Type	Days Open
AK2015-00001			Animal, Kennel Animal, Dog Licensing	Residential	371
<i>First Inspection</i>		F	03/13/2015		
<i>Close Case</i>		P	05/23/2015		
Case Number	Address	Owner	Violation	Property Type	Days Open
AK2015-00002			Animal, Kennel Animal, Dog Licensing	Residential	358
<i>First Inspection</i>		F	04/03/2015		
<i>Close Case</i>		P	04/17/2015		
Case Number	Address	Owner	Violation	Property Type	Days Open
AK2015-00003			Animal, Kennel Animal, Dog Licensing	Residential	306
<i>First Inspection</i>		F	05/25/2015		
<i>Close Case</i>		P	10/27/2015		

Animal, reckless

Case Number	Address	Owner	Violation	Property Type	Days Open
AR2015-00001			Animal, Reckless Animal, Farm Animal	Residential	182
<i>First Inspection</i>		F	09/26/2015		
<i>Close Case</i>		P	10/25/2015		
Case Number	Address	Owner	Violation	Property Type	Days Open
AR2015-00002			Animal, Reckless	Residential	100
<i>First Inspection</i>		F	12/17/2015		
<i>Close Case</i>		P	01/20/2016		

Commercial vehicles

Case Number	Address	Owner	Violation	Property Type	Days Open
CV2016-00001			Commercial Vehicles	Residential	43
<i>First Inspection</i>		P	02/12/2016		
<i>Close Case</i>		P	02/12/2016		

Debris or Junk

Case Number	Address	Owner	Violation	Property Type	Days Open
DJ2015-00001			Debris or Junk Vehicles	Residential	372
<i>First Inspection</i>		F	04/07/2015		
<i>Close Case</i>		P	11/09/2015		
Case Number	Address	Owner	Violation	Property Type	Days Open
DJ2015-00002			Debris or Junk	Residential	364
<i>First Inspection</i>		F	02/10/2015		
<i>Close Case</i>		P	02/14/2016		
Case Number	Address	Owner	Violation	Property Type	Days Open
DJ2015-00003			Debris or Junk	Residential	358
<i>First Inspection</i>		F	04/03/2015		
<i>Close Case</i>		P	05/27/2015		

Grass

Case Number	Address	Owner	Violation	Property Type	Days Open
GR2015-00003			Grass	Residential	300
<i>First Inspection</i>		F 06/09/2015			
<i>Close Case</i>		P 07/04/2015			

Case Number	Address	Owner	Violation	Property Type	Days Open
GR2015-00005			Grass	Residential	209
<i>First Inspection</i>		F 08/30/2015			
<i>Close Case</i>		P 09/30/2015			

No permit

Case Number	Address	Owner	Violation	Property Type	Days Open
NP2016-00001			No permit No permit, Accessory	Residential	50
<i>First Inspection</i>		P 10/21/2015			
<i>Close Case</i>		P 10/21/2015			

No permit, Home Occupation

Case Number	Address	Owner	Violation	Property Type	Days Open
PH2015-00001			Vehicles Debris or Junk	Residential	364
<i>First Inspection</i>		F 03/24/2015			
<i>Close Case</i>		P 05/20/2015			

Case Number	Address	Owner	Violation	Property Type	Days Open
PH2015-00002			No permit, Home Occupation Vehicles	Residential	331
<i>First Inspection</i>		P 04/30/2015			
<i>Close Case</i>		P 04/30/2015			

Case Number	Address	Owner	Violation	Property Type	Days Open
PH2015-00003			No permit, Home Occupation Vehicles Debris or Junk	Residential	309
<i>First Inspection</i>		F 05/22/2015			
<i>Close Case</i>		P 09/25/2015			

Case Number	Address	Owner	Violation	Property Type	Days Open
PH2015-00004			No permit, Home Occupation Commercial Vehicles	Residential	224
<i>First Inspection</i>		F 08/15/2015			
<i>Close Case</i>		P 11/21/2015			

Noise

Case Number	Address	Owner	Violation	Property Type	Days Open
NO2015-00001			Noise	Residential	306
<i>First Inspection</i>		F 05/25/2015			
<i>Close Case</i>		P 06/24/2015			

Public Nuisance

Case Number	Address	Owner	Violation	Property Type	Days Open
PN2015-00002			Accessory Structure Public Nuisance	Residential	174
<i>First Inspection</i>		F 10/04/2015			
<i>Close Case</i>		P 11/04/2015			

SEPTIC

Case Number	Address	Owner	Violation	Property Type	Days Open
ST2015-00003			Septic	Residential	364
			<i>Non-Compliant Septic 1st Notice</i>		
			<i>Close Case</i>		

Case Number	Address	Owner	Violation	Property Type	Days Open
ST2015-00005			Septic	Residential	364
			<i>Non-Compliant Septic 1st Notice</i>		
			<i>Close Case</i>		

Case Number	Address	Owner	Violation	Property Type	Days Open
ST2015-00006			Septic	Residential	364
			<i>Non-Compliant Septic 1st Notice</i>		
			<i>Close Case</i>		

Case Number	Address	Owner	Violation	Property Type	Days Open
ST2015-00007			Septic	Residential	364
			<i>Non-Compliant Septic 1st Notice</i>		
			<i>Close Case</i>		

Case Number	Address	Owner	Violation	Property Type	Days Open
ST2015-00011			Septic	Residential	350
			<i>Non-Compliant Septic 1st Notice</i>		
			<i>Close Case</i>		

Case Number	Address	Owner	Violation	Property Type	Days Open
ST2015-00016			Septic	Residential	345
			<i>Non-Compliant Septic 1st Notice</i>		
			<i>Close Case</i>		

SEPTIC, IMMINENT HEALTH THREAT

Case Number	Address	Owner	Violation	Property Type	Days Open
SI2015-00002			Septic, Imminent Health	Residential	370
			<i>Imminent Health Threat 1st Notice</i>		
			<i>Close Case</i>		

Case Number	Address	Owner	Violation	Property Type	Days Open
SI2015-00003			Septic, Imminent Health	Residential	364
			<i>Imminent Health Threat 1st Notice</i>		
			<i>Close Case</i>		

Case Number	Address	Owner	Violation	Property Type	Days Open
SI2015-00004			No permit	Residential	348
			<i>Imminent Health Threat 1st Notice</i>		
			<i>Close Case</i>		

Vehicles

Case Number	Address	Owner	Violation	Property Type	Days Open
VE2015-00002			Vehicles Debris or Junk	Residential	358
			<i>First Inspection</i>		
			<i>Close Case</i>		

Complaints reported

Accessory Structure	2
Animal Licensing, Dog	5
Animal Licensing, Farm Animals	5
Animal Noise	2
Animal, Kennel	4
Animal, reckless	2
Animals, number of	1
Commercial vehicles	2
Debris or Junk	17
Fence	3
Grass	3
Hazardous Property	1
No permit	3
No permit, Home Occupation	6
Noise	1
Public Nuisance	3
Rental Complaint	3
SEPTIC	34
SEPTIC, IMMINENT HEALTH THREAT	5
Unlawful Occupation of a Building	2
Vehicles	28
Total:	132

